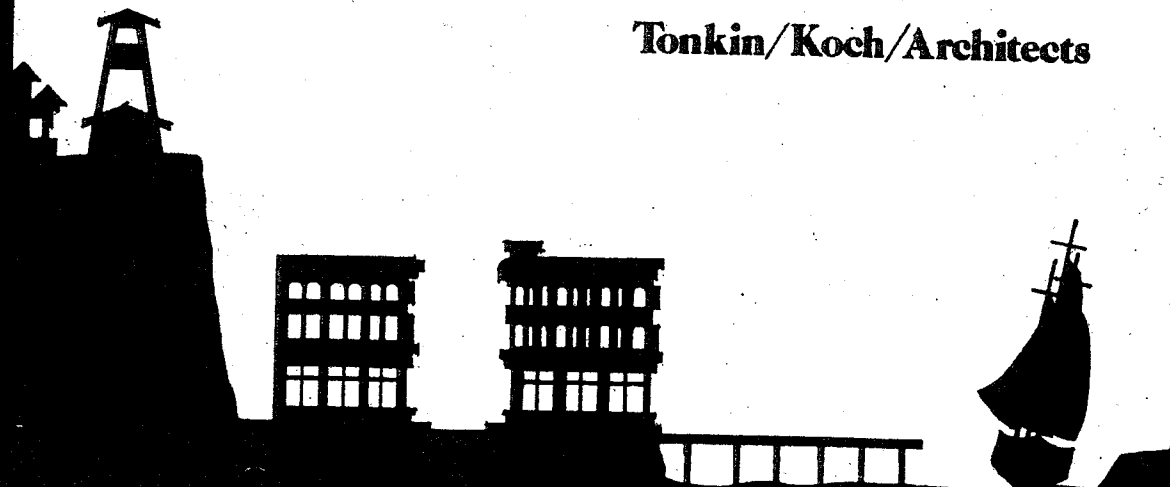


FOR PUBLIC REVIEW



STREETSCAPE DESIGN

Tonkin/Koch/Architects

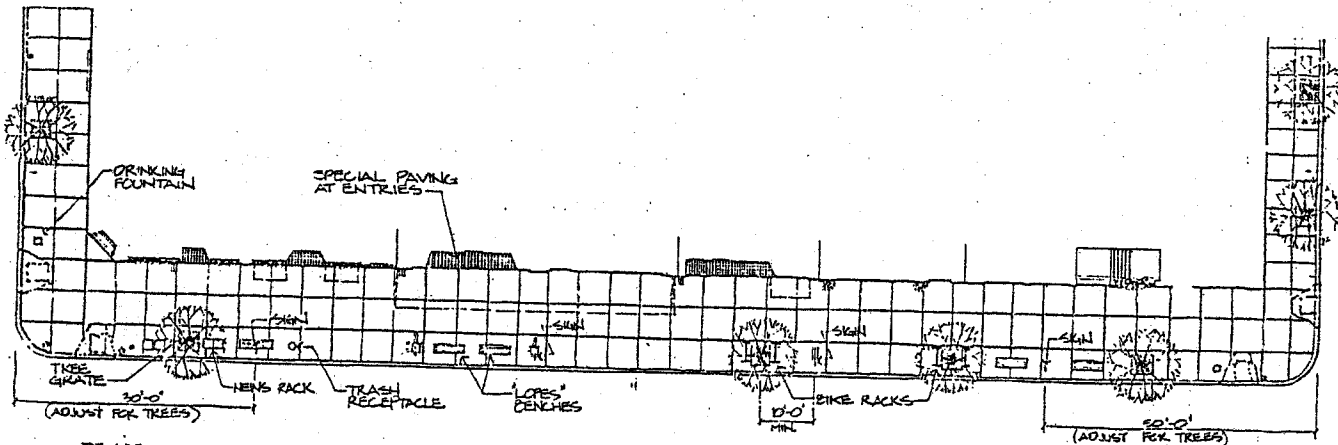


PORT TOWNSEND HISTORIC DISTRICT

STREETSCAPE DESIGN



ELEVATION



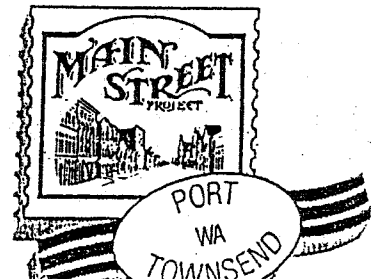
PLAN

PORT TOWNSEND MAIN STREET PROJECT

Tonkin/Koch/Architects Inc. ps

204 FIRST AVENUE SOUTH SEATTLE, WASHINGTON 98104
206 624-7880

Principal-in-charge Les Tonkin
Project Manager Edward Carr
Graphics John McLaren



December 16, 1987

Coila Sheard, Design Committee Chairperson
Port Townsend Main Street Project
540 Water Street
Port Townsend, WA 98368

Dear Coila:

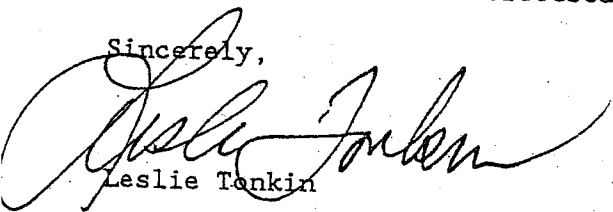
It is my very great pleasure to present the final draft of the Port Townsend Streetscape Design. We have incorporated sidewalk and street tree design details and specifications.

The booklet is intended to serve as a design guide and technical reference for future changes and additions to the Port Townsend Historic District. Adherence and use of the design guidelines will serve to promote the preservation of Water Street's past historic legacy and to reinforce the good design elements that have evolved recently.

Our design recommendations are intended to build on and to strengthen the context of the already wonderful environment of the Water Street area. Many of our recommendations are based on selecting the best examples of existing situations or taking design committee and staff recommendations or those of local merchants and building owners. The design guide uniquely reflects the Port Townsend context and character.

On behalf of my staff and myself, I would like to extend our appreciation to you, David Kahley, members of the Main Street Board of Directors, Mayor Shirley, and the City Council for the opportunity to work with you on this planning document. I am always available to assist you and the City in the implementation of the Streetscape Design.

Sincerely,



Leslie Tonkin

LT:tw

ACKNOWLEDGMENTS

The Streetscape Design was commissioned by the Design Committee of the Port Townsend Main Street Project.

Major funding for the Streetscape Design was made possible from a professional services grant awarded to the Port Townsend Main Street Project by the Washington State Department of Community Development.

Additional funding was provided by the City of Port Townsend.

Port Townsend Main Street Project:

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Coila Sheard, Chairperson
Tom Johnson
Sally Boyer
Lynne Reid

Main Street Board of Directors:

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John Clise	Julie McCulloch	Gary Swett
Leon Crowl	Lynne Reid	Bill White
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INTRODUCTION

The Port Townsend Water Street Historic District Main Street Program asked Tonkin/Koch/Architects for a streetscape design for Water Street. It includes:

- the specification of streetscape accessories
- sidewalk standards
- the design of a street sign face unique to the historic district.

This set of recommendations can be used to guide all streetscape work on Water Street.

As Port Townsend's Water Street Historic District continues to grow as an active pedestrian district, it is increasingly necessary to improve the pedestrian environment. This report lists the factors involved in improving the pedestrian environment, and makes recommendations based on the factors.

The following factors are considered in detail:

- Sidewalk zoning
- Sidewalk furnishings and amenities
- Sidewalk paving patterns
- Street sign improvements

STREET ACCESSORIES

The concept used in recommending sidewalk furnishings and accessories is that new work in historic districts must respond sensitively to local visual and textural context, without competing with it. New work should not be too ornate and busy, and should be discrete in attempts to imitate historic detail.

The report sets out guidelines for the installation of street accessories, distilled from a study of sidewalk zoning. The report does not consider major changes, such as making new sidewalk lobes at the street corners. Nor is the intent of the report to lay out a preliminary plan for a total street and sidewalk improvements project. But the "typical block plan" found within can be applied on a block by block basis to become an improvement plan if the city wishes. If so used, it must be realized that the report does not consider every possible street accessory. Other street accessories should not be barred from the streets on the basis of this report. The principles of sidewalk zoning developed in this report should be used to locate any accessories not considered by this report.

Some of the recommended accessories are beyond current programs for sidewalk improvements. They are included in this report to present a complete and integrated picture of what might be included in the streetscape.

SIDEWALK STANDARDS

A careful study of sidewalk zoning for pedestrian traffic and streetscape accessories results in a thorough set of recommendations for the placement of all accessories. Paving patterns are rationalized.

SIGN STUDY

The location, number and type of street signs for parking, traffic regulation, and for information in Port Townsend need improvement. The proposed redesign of the street sign

- strengthens graphic content
- creates an attractive sign
- incorporates a symbol unique to Port Townsend.

Several different symbols were studied:

- A town section cut through the district. Such a section incorporates the most important elements in the Water Street Historic District: The residential area overlooking the district, the bluff, Water Street bounded on each side by historic three and four story buildings, the waterfront wharves, a ship, and distant headlands.
- The Discovery, Captain Vancouver's ship, or another important ship or type of ship important to this area of Puget Sound.
- A historic building.
- A historic building detail.
- A historic streetscape.

The recommended sign symbol incorporates the site section cut through Port Townsend. The elements included in the graphic are not literal. Abstraction is needed to simplify the image for reproduction in the figure/ground relation of a street sign. It is recommended because:

- It is the only symbol under study both unique to Port Townsend and expressible in a figure/ground graphic.
- In the overall graphic design of the sign it does not compete with the message.

- The bluff/mainstreet/waterfront is a fitting and brief visual description of the essence of the Water Street Historic District. Emphasis is on the large scale elements that define the district.
- The "main street" in the symbol is a reference to the Main Streets program itself.

The existing parking signs have a major graphic weakness. The light blue lettering on a white background lacks the color contrast necessary for legibility. We recommend that the standard highway sign colors and alphabet be used on the regulatory and information signs, with one color used for both the lettering and border per sign. The town section, growing out of the border, will be in the color of the border and the lettering. Parking regulation signs should be standard green on a white ground; parking prohibition signs should be standard red on a white ground; parking area directional signs should be standard green on a white ground; pullover directional signs should be white on blue; and recreational and cultural interest signs should be white on green or white on brown. The specifications for colors is included.

METHODS OF STUDY

The existing conditions in the Historic District were studied and photographed. Research for the specifications of the street accessories was conducted through manufacturer's literature and through discussions with local suppliers and manufacturers. Earlier studies of Port Townsend were consulted, as well as federal regulatory literature:

A Workbook for the Port Townsend Urban Waterfront, by Kasprisin, Bettinari Design, 1980.

Field Report on Water Street Historic District, by The National Trust for Historic Preservation, 1977.

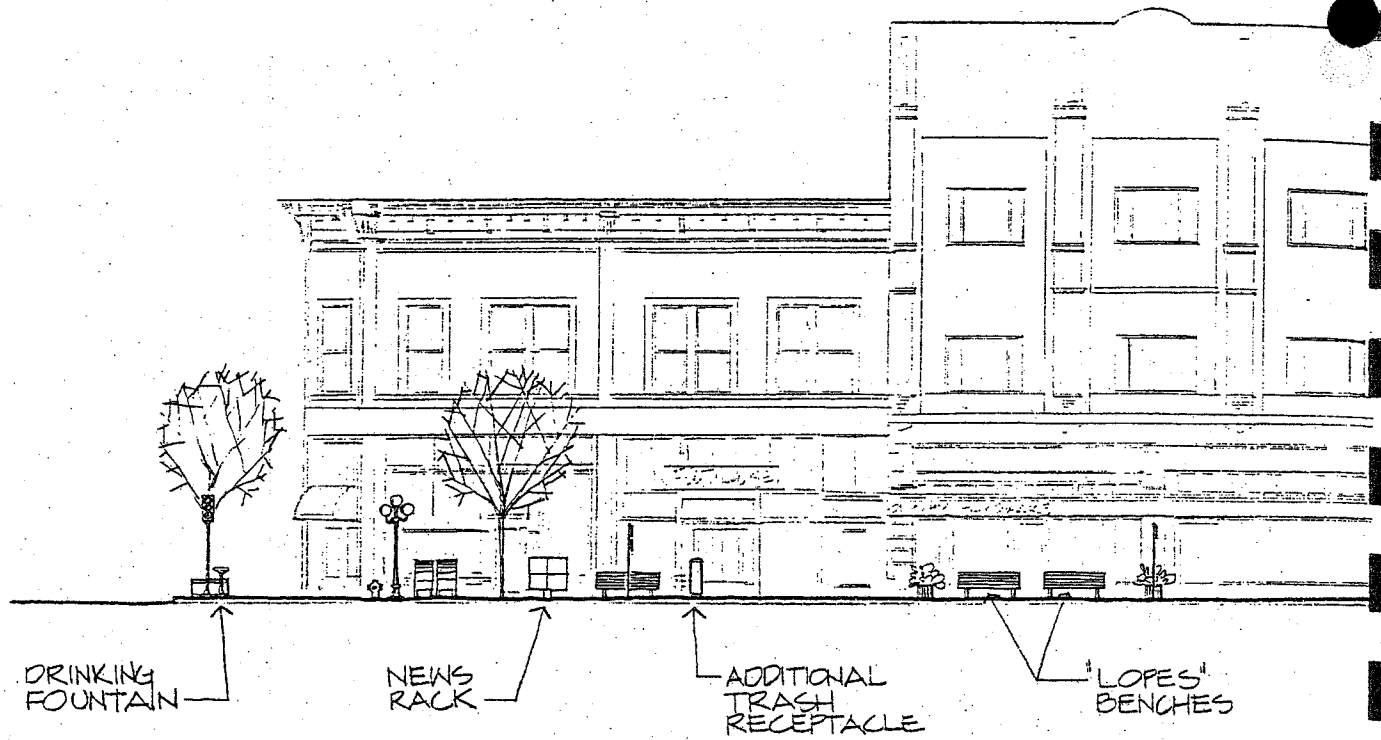
Port Townsend Zoning Ordinance (Title 17)

Manual on Traffic Control Devices, Federal Highway Administration, 1987.

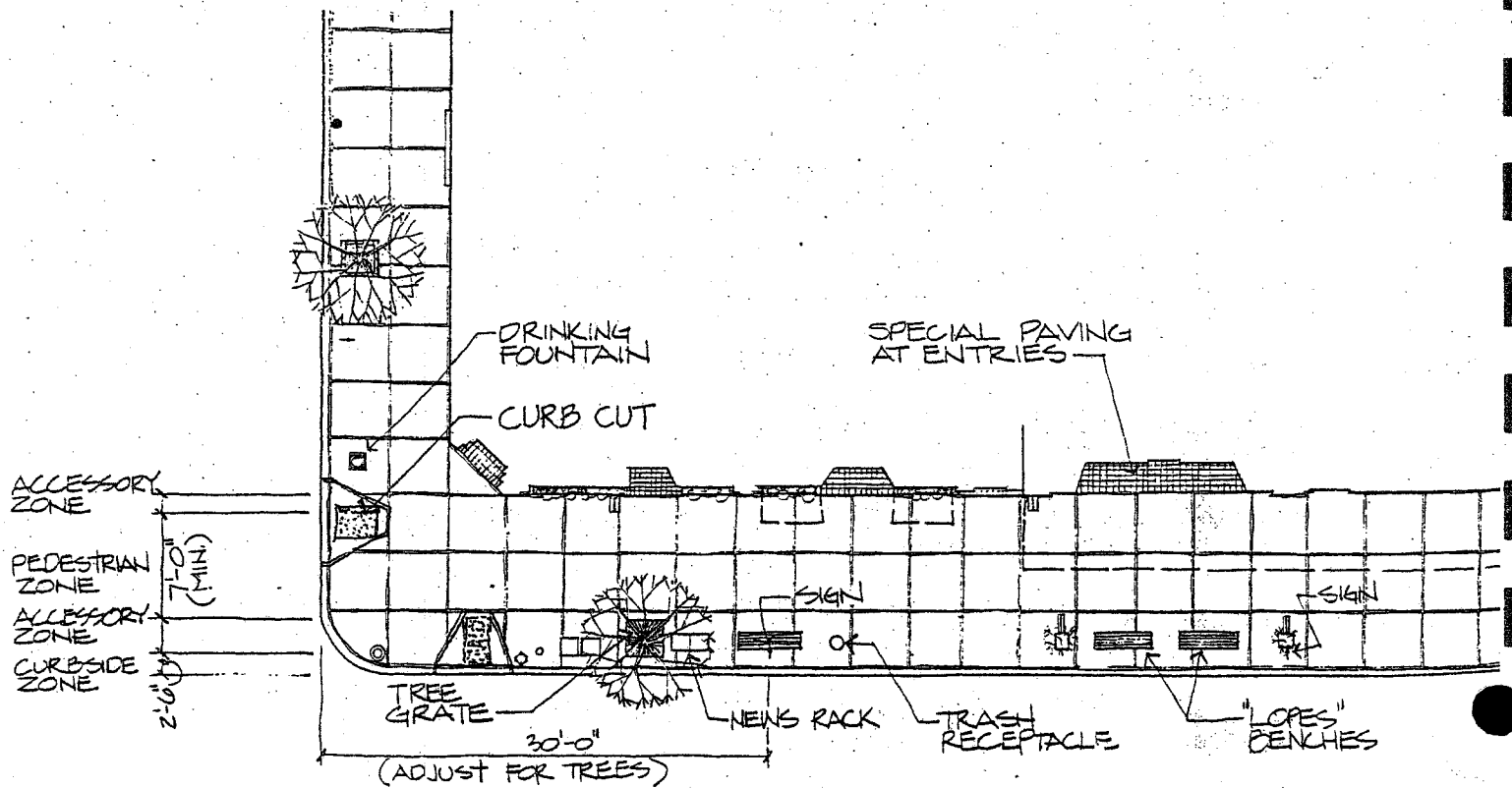
Standard Alphabets for Highway Signs, Federal Highway Administration, 1966.

Specifications for Standard Highway Sign Colors, Federal Highway Administration, 1979.

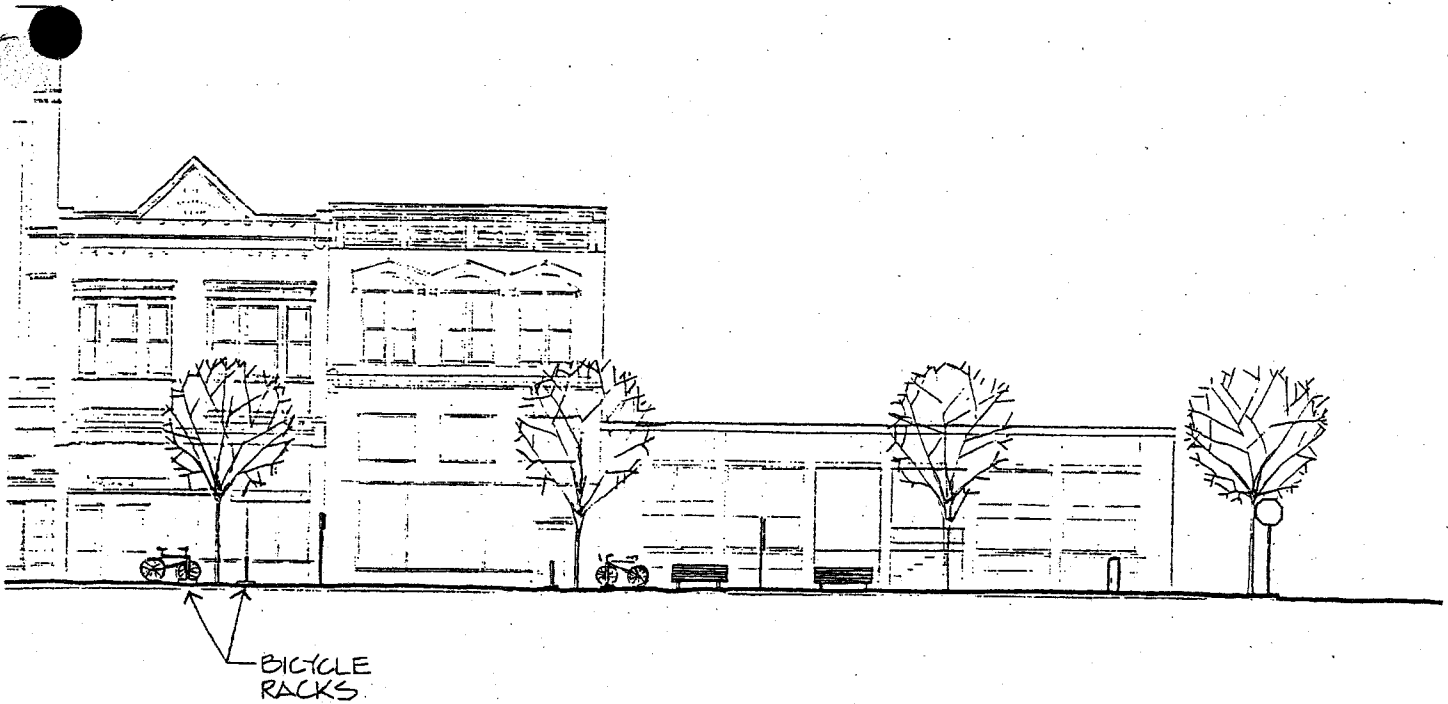
Illustrated Handbook for Barrier Free Design, Washington State Building Code Advisory Council, 1978.



ELEVATION



PLAN

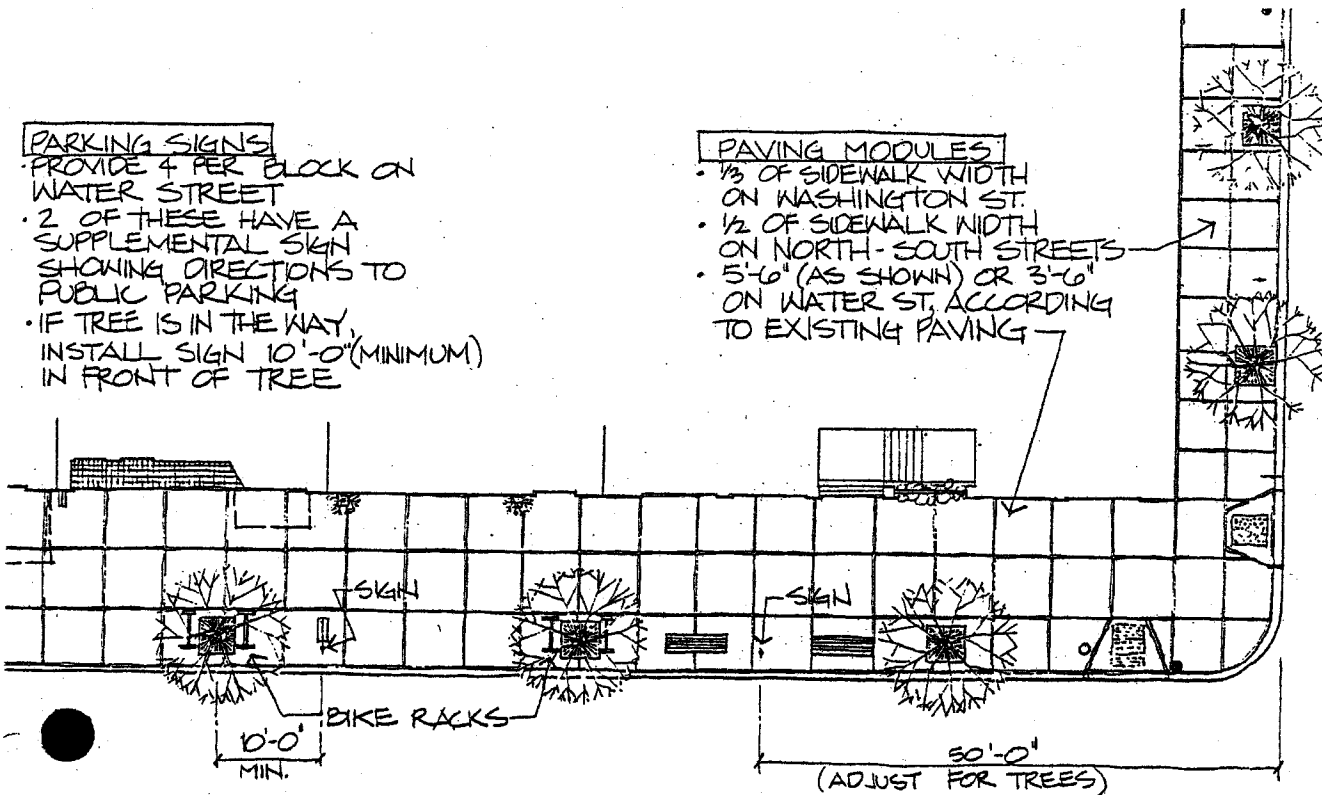


PARKING SIGNS

- PROVIDE 4 PER BLOCK ON WATER STREET
- 2 OF THESE HAVE A SUPPLEMENTAL SIGN SHOWING DIRECTIONS TO PUBLIC PARKING
- IF TREE IS IN THE WAY, INSTALL SIGN 10'-0" (MINIMUM) IN FRONT OF TREE

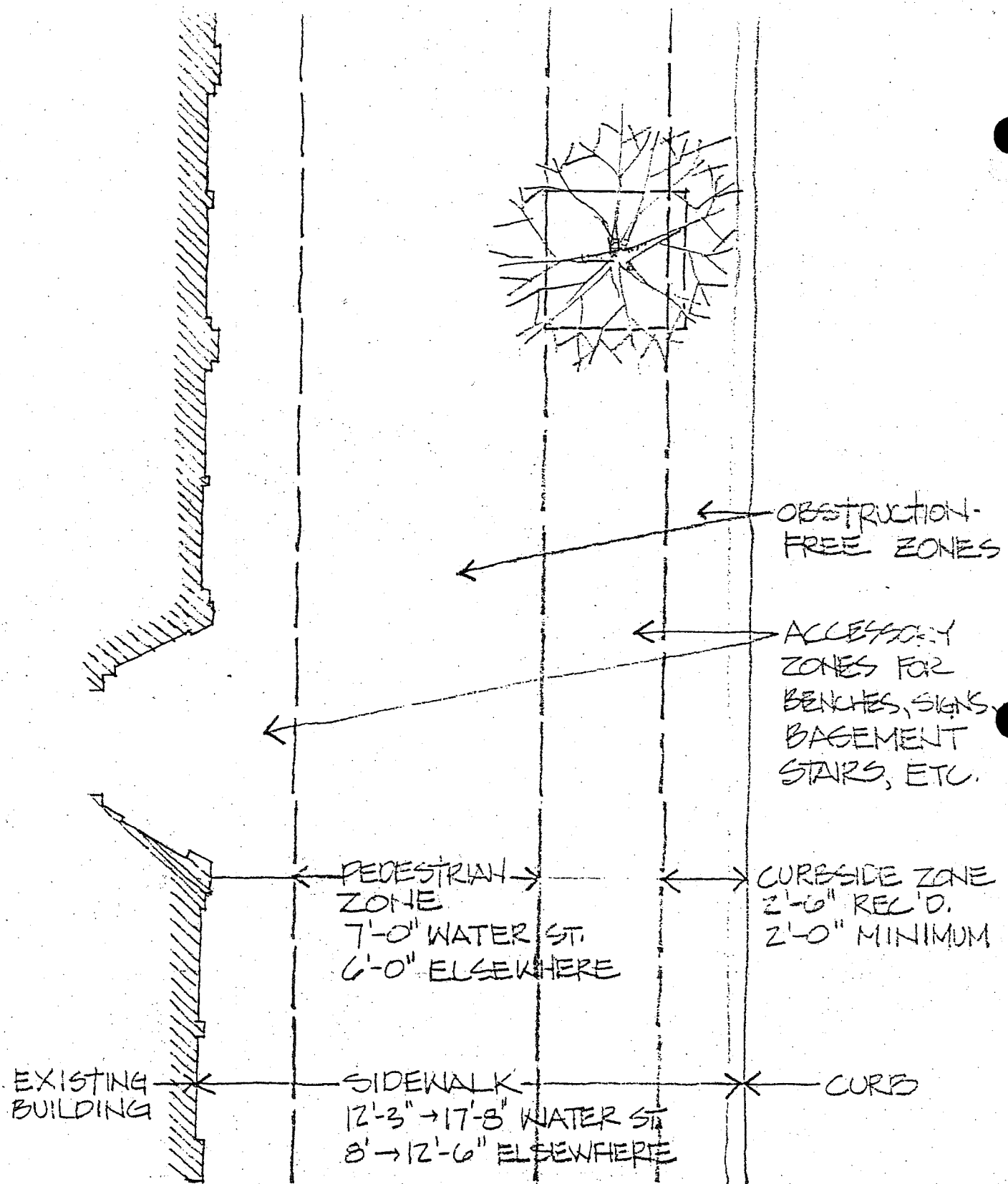
PAVING MODULES

- 1/3 OF SIDEWALK WIDTH ON WASHINGTON ST.
- 1/2 OF SIDEWALK WIDTH ON NORTH-SOUTH STREETS
- 5'-6" (AS SHOWN) OR 3'-6" ON WATER ST, ACCORDING TO EXISTING PAVING



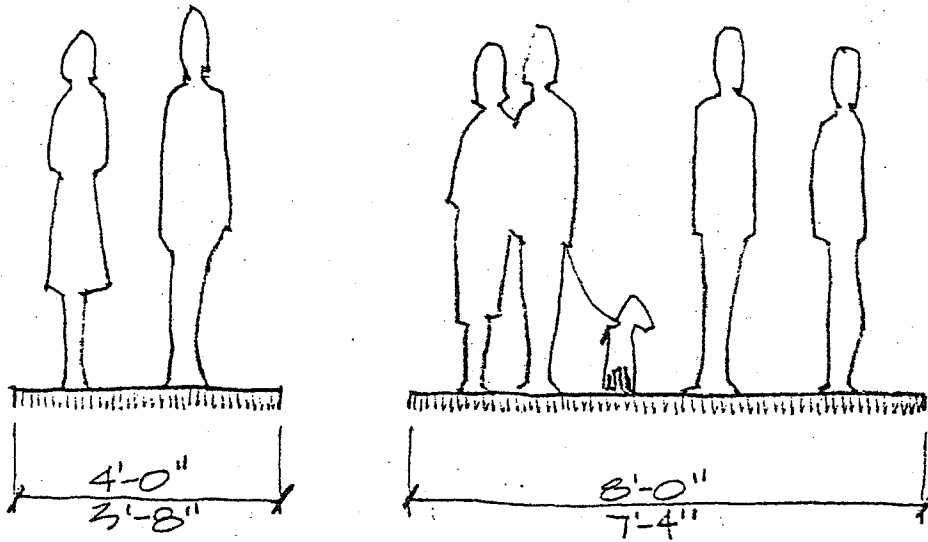
TYPICAL BLOCK

BASED ON WATER STREET BETWEEN TAYLOR & ADAMS



TYPICAL SIDEWALK PLAN

SIDEWALK ZONING

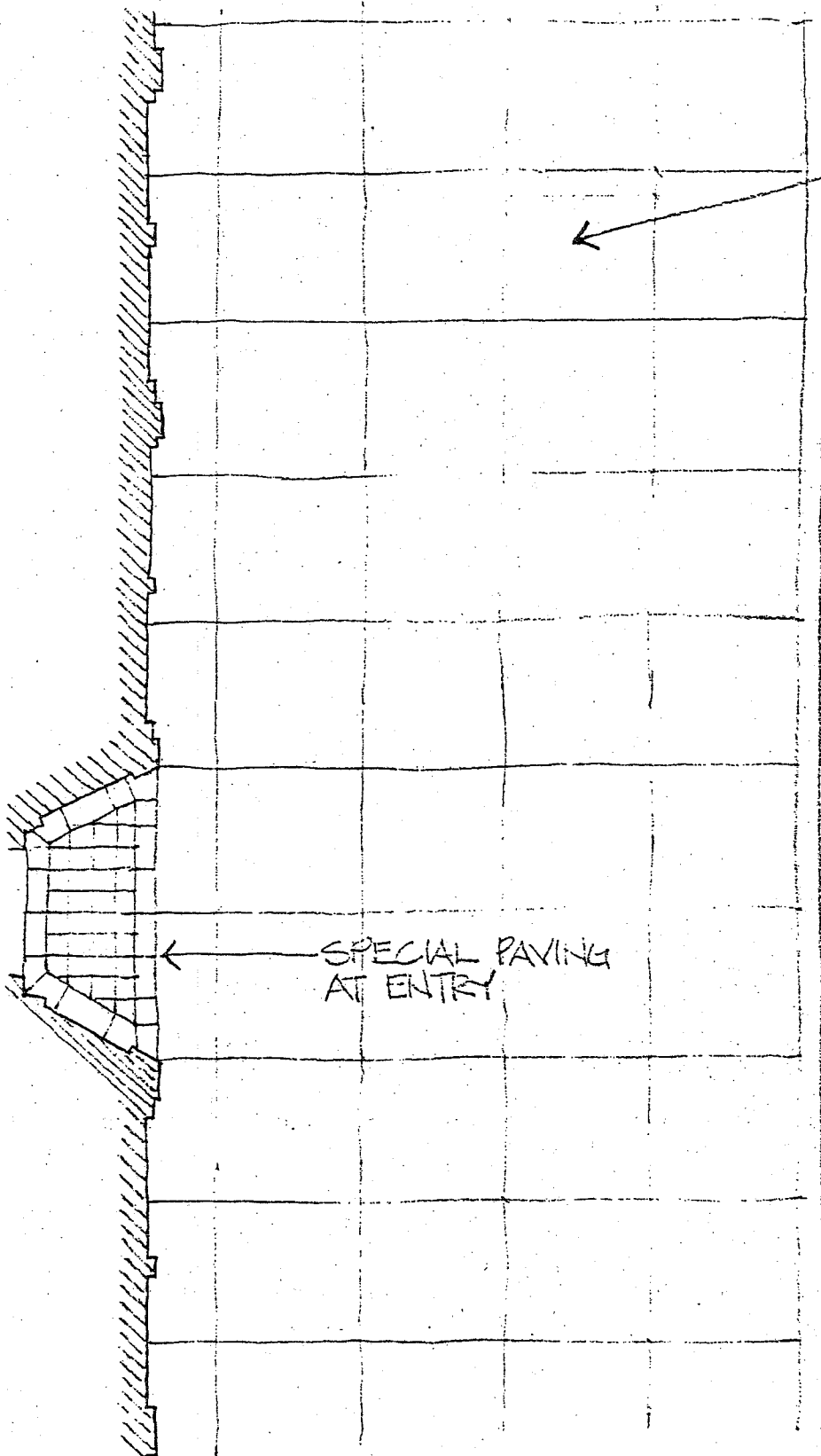


SOURCE : ARCHITECTURAL GRAPHIC STANDARDS

The urban sidewalk has four zones. Two zones must be free from all obstructions. The main zone is the pedestrian zone, which should be at least 7'-0" wide on principal sidewalks. The other obstruction-free zone is immediately adjacent to the curb, and should be a minimum of 2'-6" wide, in order to allow room for car doors and passage to and from parked cars. The combined obstruction-free width is 9'-6". Since the principal sidewalks (on Water Street) are from 12'-3" to 17'-6" wide, there is a total of from 2'-9" to 8'-0" of additional width for planters, signs, benches, and other furnishings.

One accessory zone is formed between the curbside and pedestrian zones, and a second accessory zone is bounded by the pedestrian zone and the buildings. The accessory zone at the building is eliminated at the narrow sidewalks, and the accessory zone between curbside and pedestrians varies in size with the sidewalk width.

SIDEWALK ZONING



MODULE SIZES:

- 3'-6" OR 5'-6" ON WATER STREET
- 1/2 x WIDTH OF SIDEWALK ON NORTH-SOUTH STREETS
- 1/3 x WIDTH OF SIDEWALK ON WASHINGTON STREET

SPECIAL PAVING AT ENTRY

TYPICAL SIDEWALK PLAN

1/4" = 1'-0"

SIDEWALK PAVING

The sidewalk paving pattern should be in scale with the storefronts and street furnishings. It should be simple, clean, economical, and not too busy. Concrete paving with a pattern created only by the minimum requirements for control and expansion joints has a pattern that is too large for the scale of the district. At the opposite extreme, cobblestones and bricks -- which are expensive -- create too fine-grained a pattern, given the existing precedents in the district. The most appropriate pattern is to follow the local precedent of scoring the concrete with a grid of squares.

The difficulty is to determine the recommended module size, because local precedents already vary considerably from block to block. Where there is a sidewalk along Washington Street, the precedent is a square size equal to one-third the sidewalk width. Along the north-south sidewalks, the precedent is a square one-half the sidewalk width. It is recommended that these precedents be followed in the future. On Water Street, there is no longer a widespread precedent. The recommended size is the predominant one on the block in question. This will result in two variations in paving from block to block along Water Street: a 3'-6" square grid and a 5'-6" square grid.

Recessed entry ways are the only recommended location for special paving. Appropriate materials would be differently colored and scored concrete, or some kind of tile. Colors should be predominantly earth-toned. Examples of materials that are not recommended would be cobblestones, indoor-outdoor carpeting, and bright painted concrete.

New concrete work must have expansion joints at a maximum of 20-feet on center or every 400 square feet, whichever is less.

SIDEWALK PAVING

CEMENT CONCRETE SIDEWALKS

MATERIALS.

PORTLAND CEMENT CONCRETE : The concrete mix for sidewalk shall be 5-1/2 sack concrete with 3/4" or 1-1/2" aggregate. Slump of the concrete mix shall not exceed 3-1/2 inches.

JOINTS: Premolded joint filler for use in joints shall be not less than 3/8 inch in thickness and shall consist of a suitable asphalt mastic encased in asphalt-saturated paper or asphalt-saturated felt. It shall be sufficiently rigid for easy installation in summer months and not too brittle for handling in cool weather. Premolded joint filler shall be kept on a flat surface in storage before insertion in the concrete, and no warped or damaged material shall be used. Warped or damaged material shall be removed from the project.

FORMS AND FINE GRADING.

Forms shall be staked to a true line and grade. A subgrade template shall then be set upon the forms and the fine grading completed so that the subgrade will be a minimum of 3-3/8 inches below the top of the forms.

Low areas in the subgrade shall be backfilled with select materials or with suitable native material. The backfill shall then be compacted and any high areas in the subgrade shall be cut down. The subgrade shall be thoroughly dampened prior to the time the concrete is placed.

PLACING AND FINISHING CEMENT CONCRETE SIDEWALK.

The Concrete shall be spread uniformly between the forms and thoroughly compacted with a steel shod strikeboard. Through joints shall be located and constructed in accordance with drawing. In construction of through joints, the premolded joint filler shall be adequately supported until the concrete is placed on both sides of the joint. Premolded joint filler for through joints shall be positioned in true alignment at right angles to the line of the sidewalk and be normal to and flush with the surface. Where the sidewalk will be contiguous with the curb, it shall be constructed with a thickened edge as shown on the drawing.

After the concrete has been thoroughly compacted and leveled, it shall be floated with wood floats and finished at the proper time with a steel float. Joints shall be edged with a 1/4-inch radius edger and the sidewalk edges shall be tooled with a 1/2-inch radius edger. Depending on the type, the sidewalk shall be divided into panels by scoring 1/4-inch deep.

SIDEWALK CONSTRUCTION

The surface shall be brushed with a fiber hair brush of an approved type in a transverse direction except that at driveway and alley crossings it shall be brushed longitudinally.

Depressed Curb (Wheelchair Ramp). At intersections where new cement concrete sidewalks are to be constructed, the Contractor shall construct depressed curbs. The depressed curbs shall be constructed in accordance with the drawings.

CURING AND PROTECTION.

The concrete pavement shall be protected against excess loss of moisture, rapid temperature change, rain, water and mechanical injury during and immediately following the placing and finishing operations.

The concrete pavement shall be cured for the minimum number of days listed below, exclusive of the day the concrete is placed.

Portland cement	5 days
High-early-strength cement	3 days

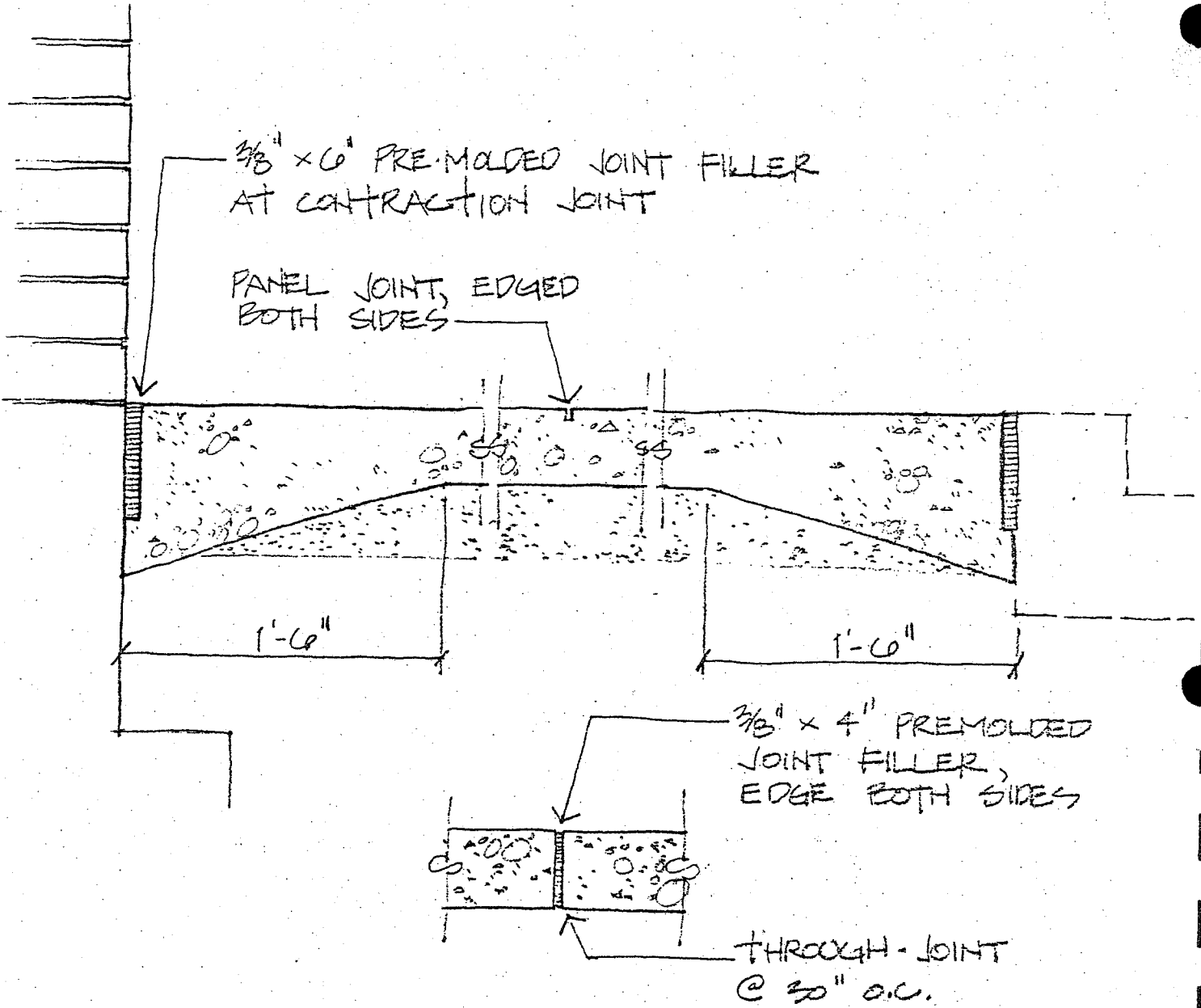
Moist curing by sprinkling or by saturated mats, waterproof paper, white polyethylene sheeting, liquid membrane or a combination of these may be used for curing medium and shall be applied in a manner and in quantity appropriate to the particular conditions. Pavement edges which are exposed by the removal of the forms shall be protected by the immediate application of a curing medium or moist earth.

All curing materials shall be free of all substances which are considered to be harmful to portland cement. The curing medium shall be capable of preventing checking, cracking and dry spots regardless of conditions existing at the time of placement.

The Contractor shall have readily available sufficient protective covering, such as waterproof paper or plastic membrane, to cover the pour of an entire day in event of rain or other unsuitable weather.

The sidewalk shall be protected against damage or defacement of any kind until it has been accepted by the Owner. Sidewalk which is not acceptable to the Owner because of damage or defacement, shall be removed and replaced at the expense of the Contractor.

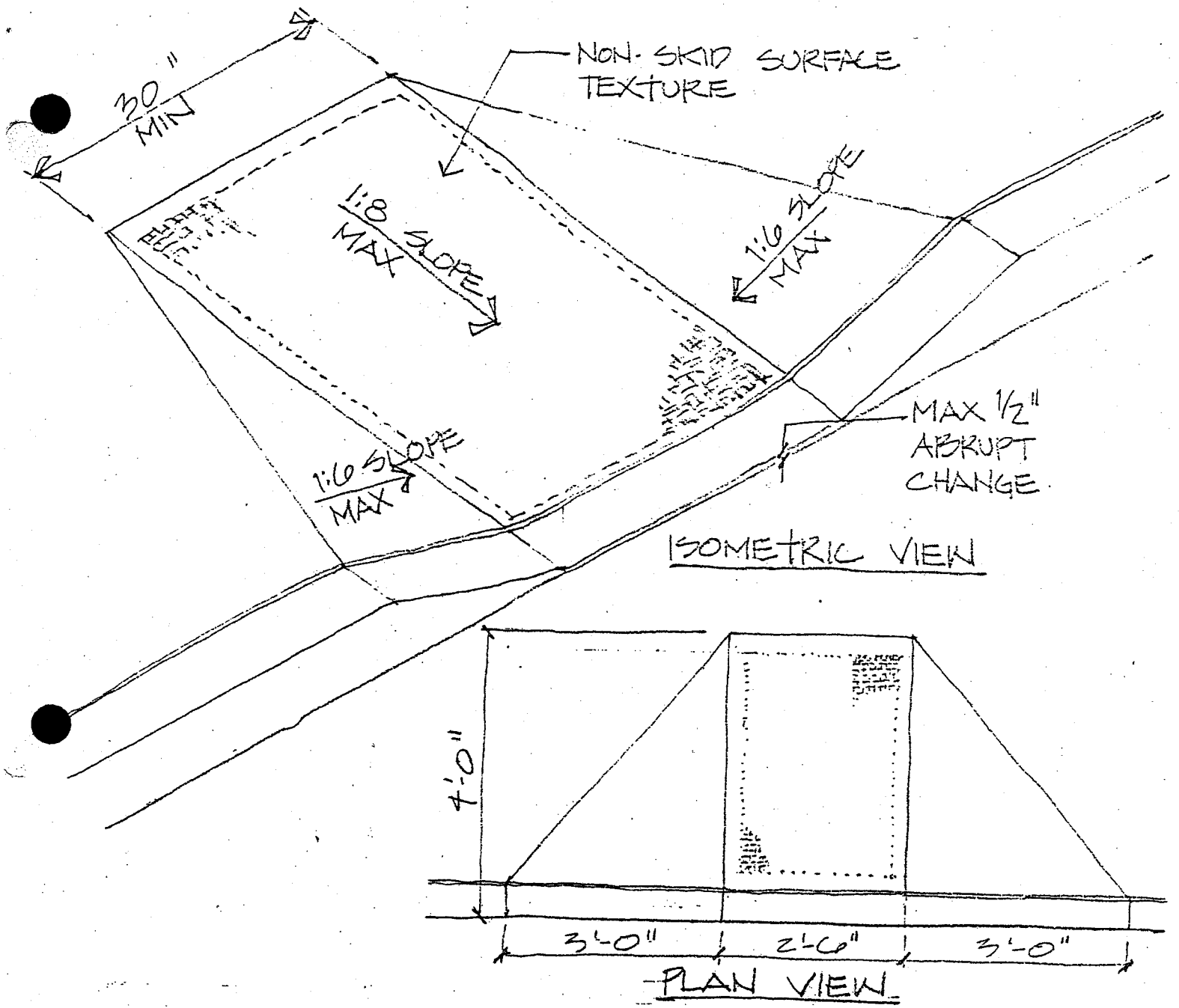
SIDEWALK CONSTRUCTION



TYPICAL SIDEWALK SECTION

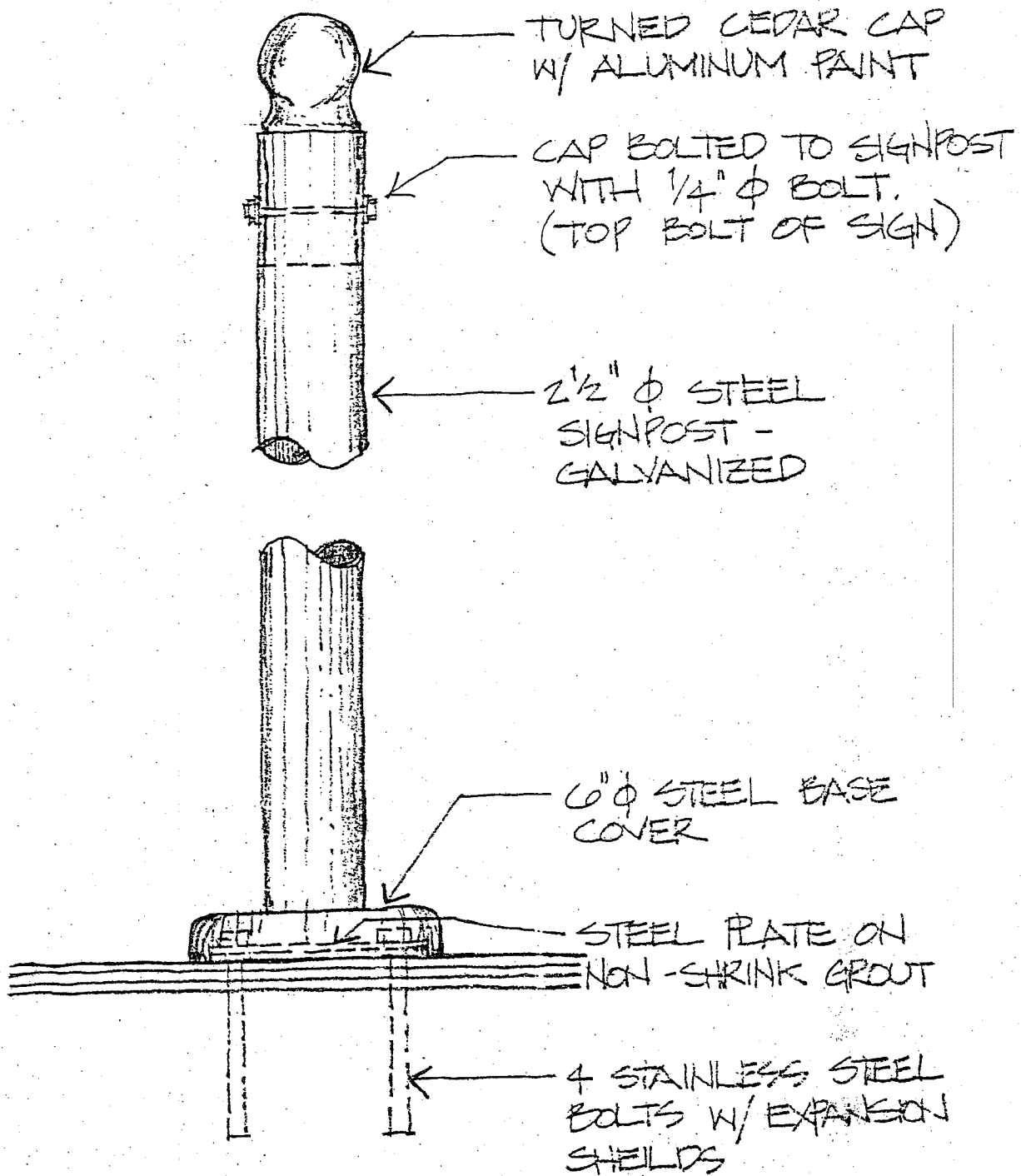
NO SCALE

SIDEWALK CONSTRUCTION

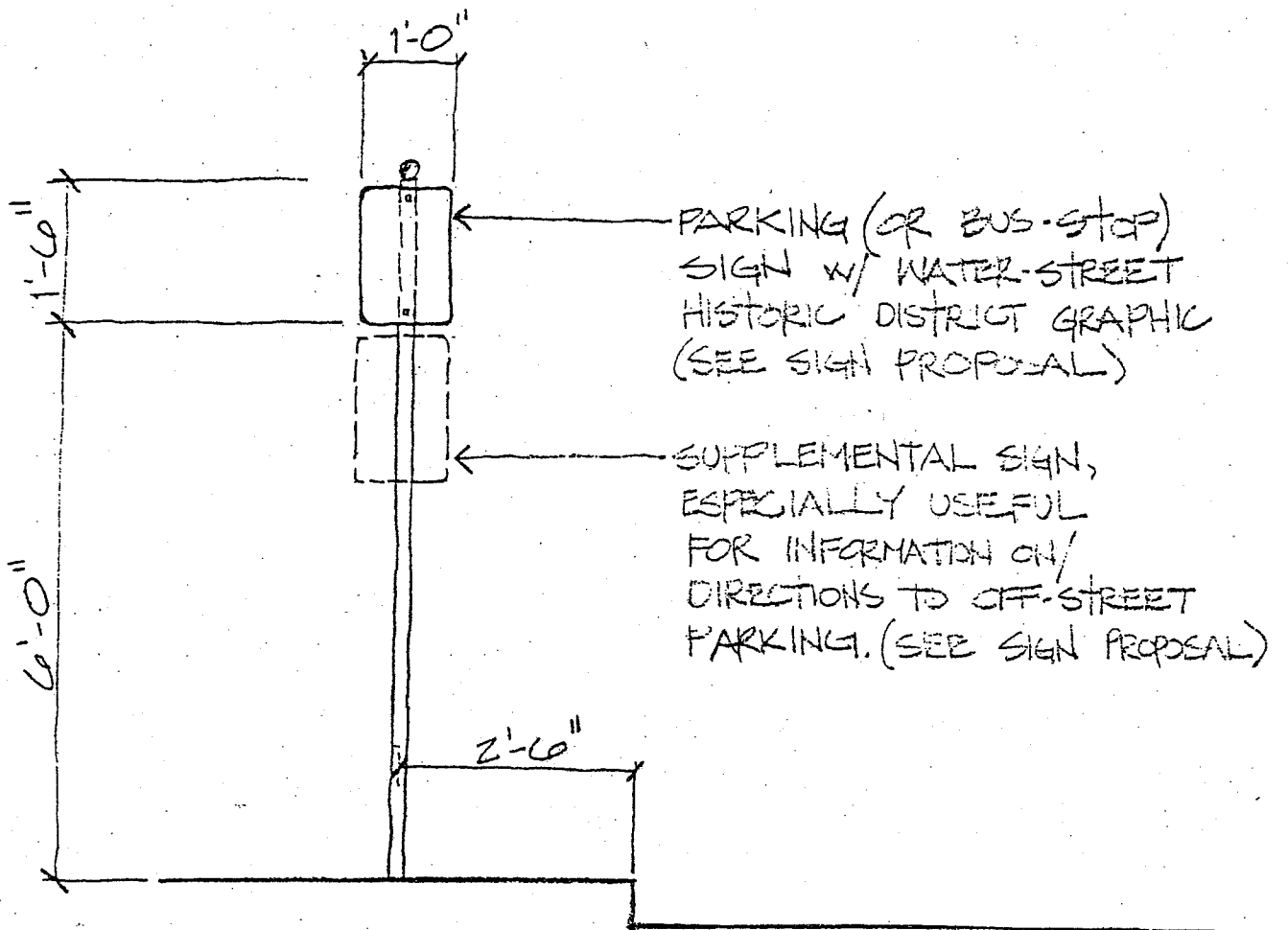


Curb cuts should be installed at each corner, as shown on the typical block plan. Minimum requirements are shown above. The special texture is required to warn persons with sight disabilities of the presence of the curb cut. A curb cut is hazardous if its lower edge is greater than 1/2-inch above the roadway. Care must also be taken to maintain curb drainage without allowing the lower part of the curb cut to be submerged.

CURB CUTS



SIGNPOST



There are a number of problems with existing parking signs:

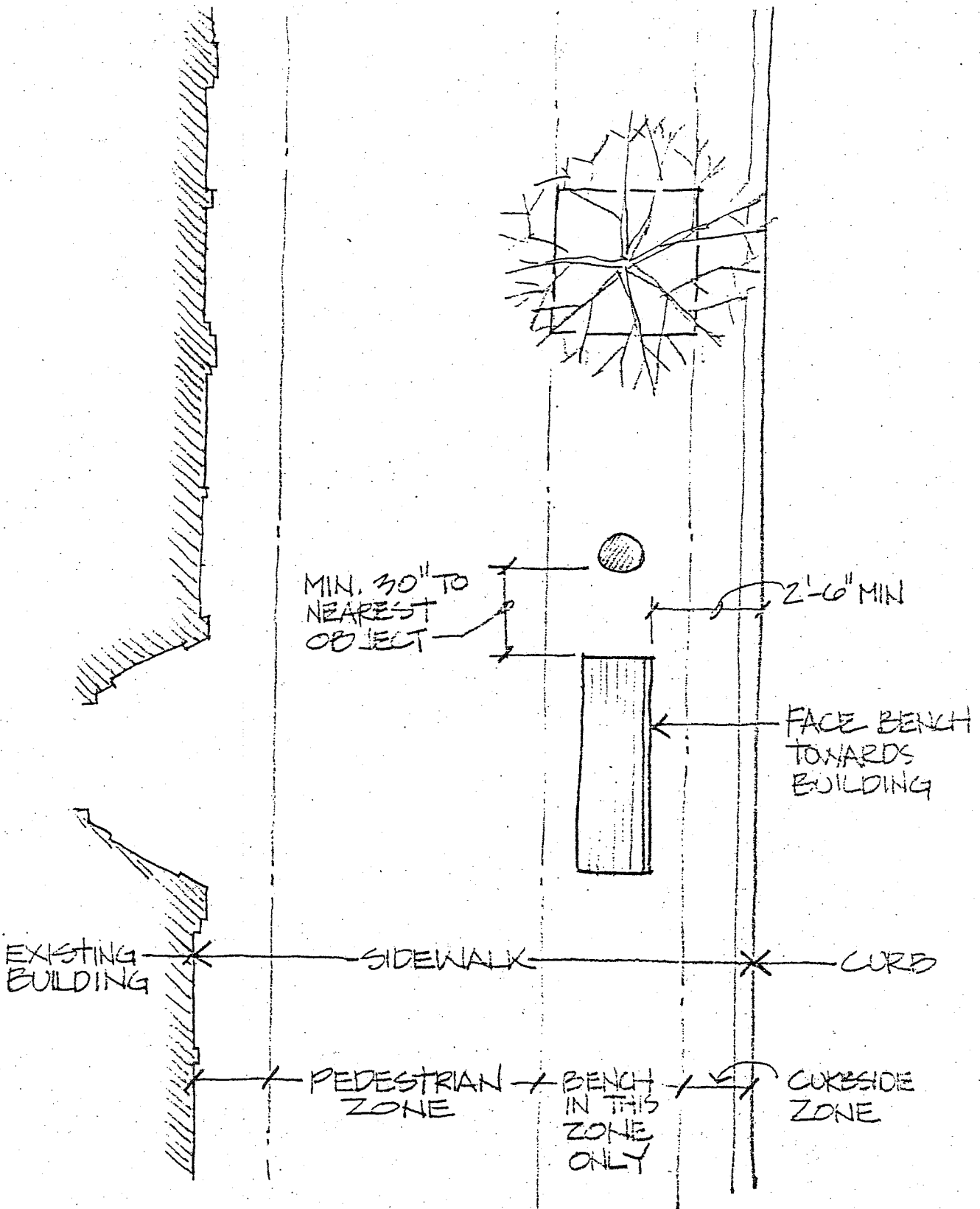
- There are not enough of them
- A few tend to get obscured by street trees
- They are graphically weak
- They do not provide enough information
- They are unattractive

As shown on the typical block plan, one way of solving the problem with the signs being obscured by street trees is to move them to new locations as shown. They may also be lowered to 6-feet as shown above, and of course, trees should be regularly pruned.

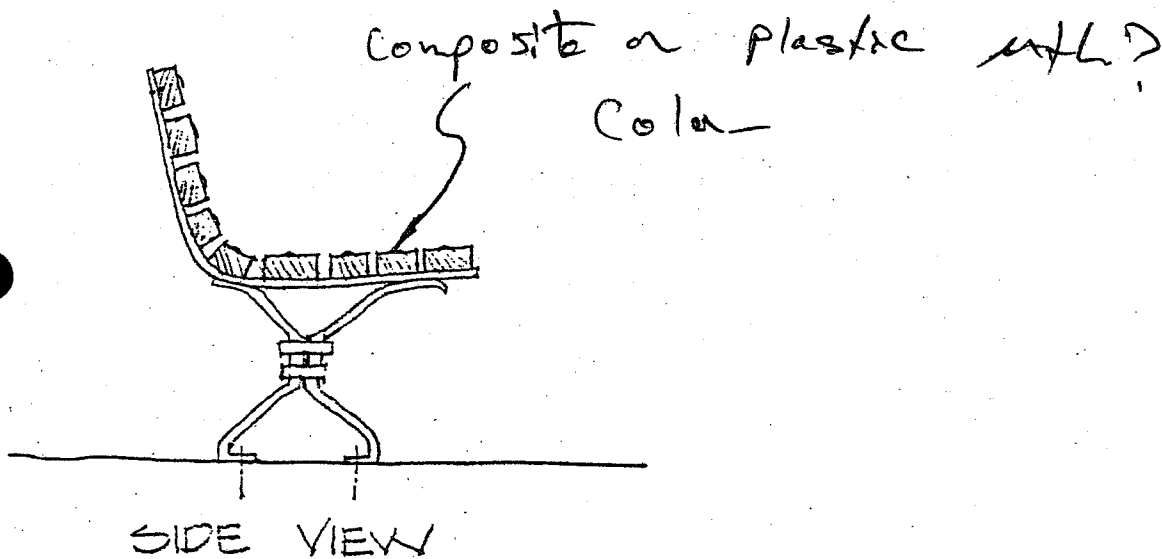
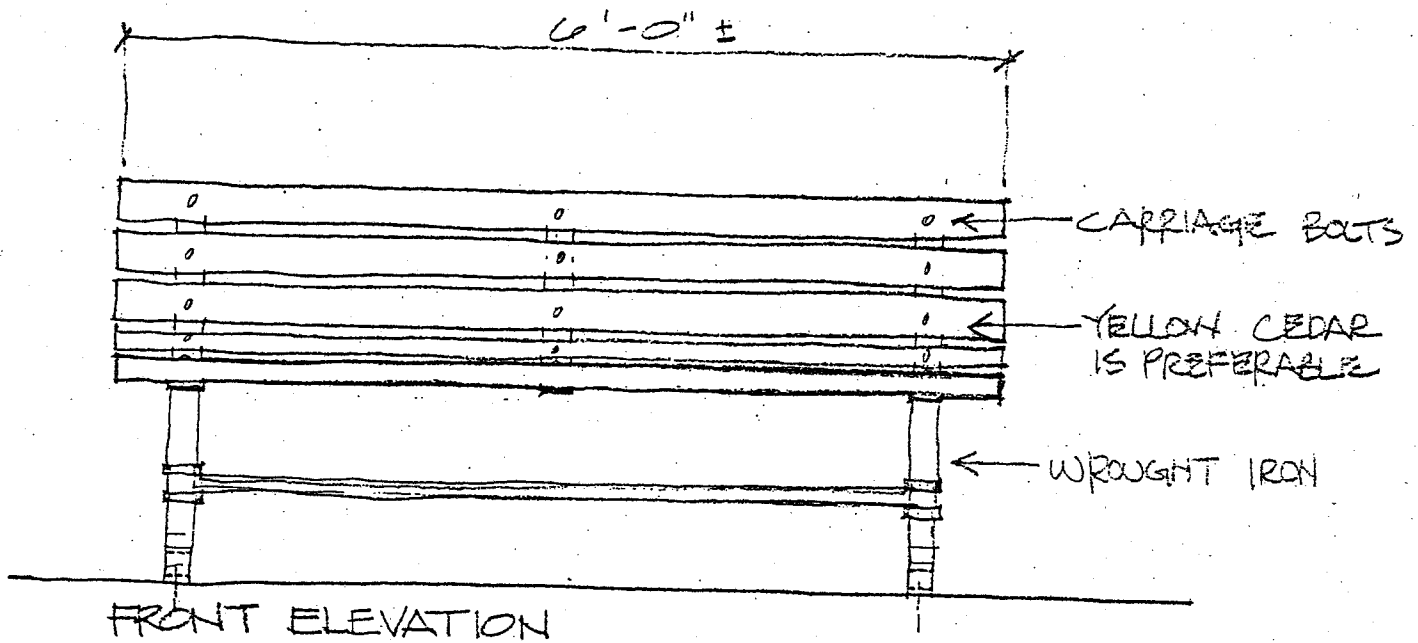
Currently, there are two or three "2-HOUR PARKING" signs per block, along Washington Street. This should be changed to four signs per block, with supplemental signs where needed. Not enough information is provided on the location of the public parking lots. Supplemental signs below the main sign will direct traffic to the public lots.

The graphic weakness of the signs is probably another reason that they are not perceived well enough. There is little contrast between lettering and background colors. The information content of the sign is poorly organized. The sign proposal section of this report includes a means of strengthening the graphic content of the sign.

REGULATORY SIGN



BENCHES



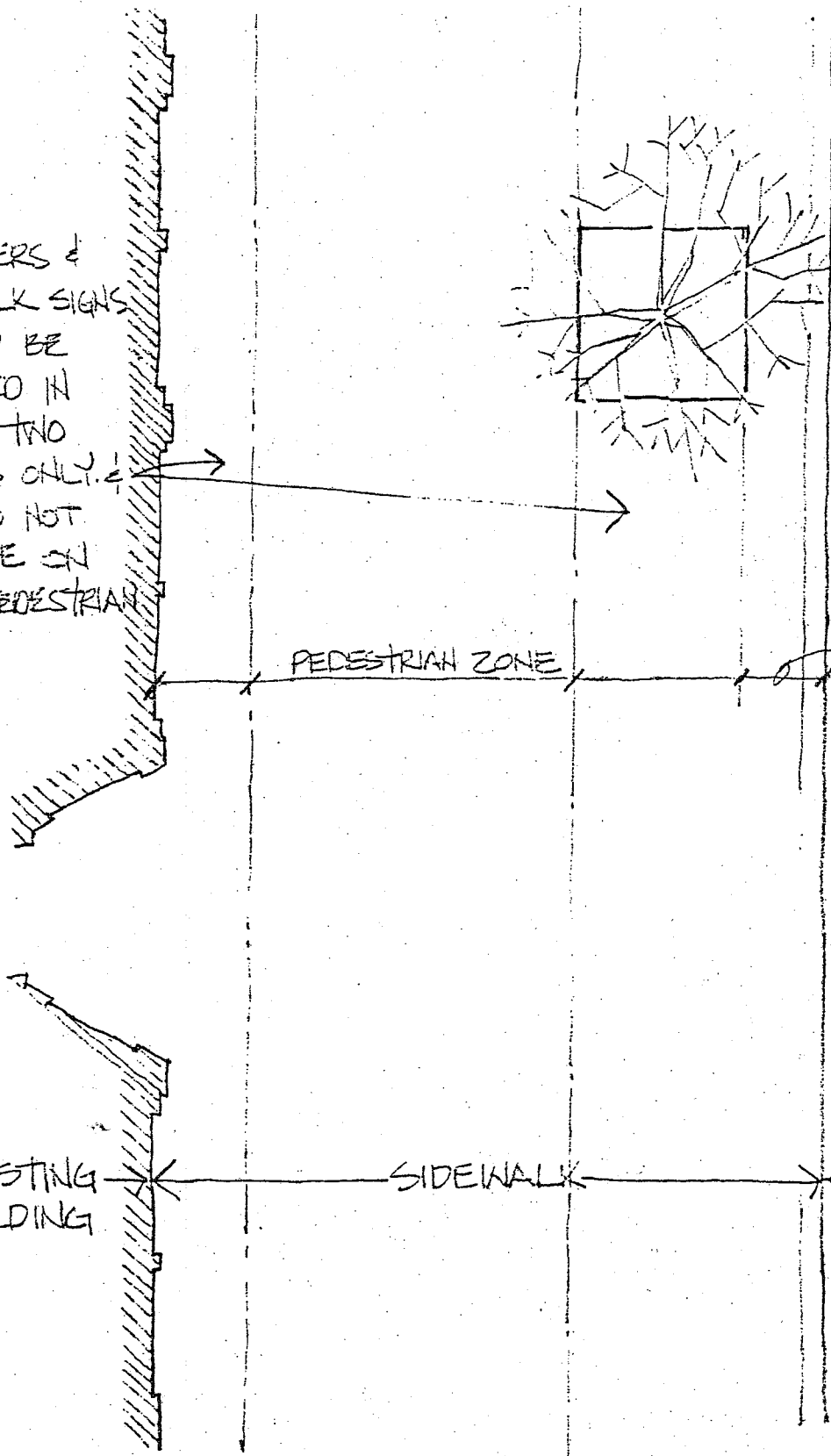
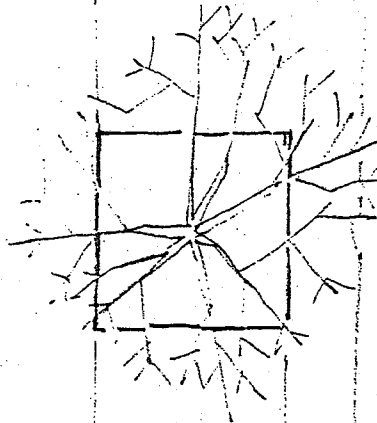
The recommended bench is the "Lopes" bench, manufactured by Steve Lopes, Blacksmith, of Port Townsend. The design approximates historic lines, yet is simple, clean, and without too much ornate detail. It is sturdy and there is additional value in its being of local design and construction. The cost compares quite favorably with the commercial models.

It is recommended that the wood seating slats be constructed of 1-1/2-inch thick wood, instead of the 1-inch thicknesses of the two prototypes currently in use on Water Street. The slats should be Alaskan Yellow Cedar, instead of the Douglas Fir used on the prototypes. In addition, it is recommended that the bench be bolted to the sidewalk, instead of cast in place.

Estimated Cost: \$450

BENCHES

PLANTERS &
SIDEWALK SIGNS
SHOULD BE
LOCATED IN
THESE TWO
ZONES ONLY.
SHOULD NOT
INTRUDE ON
THE PEDESTRIAN
ZONE



← EXISTING
BUILDING

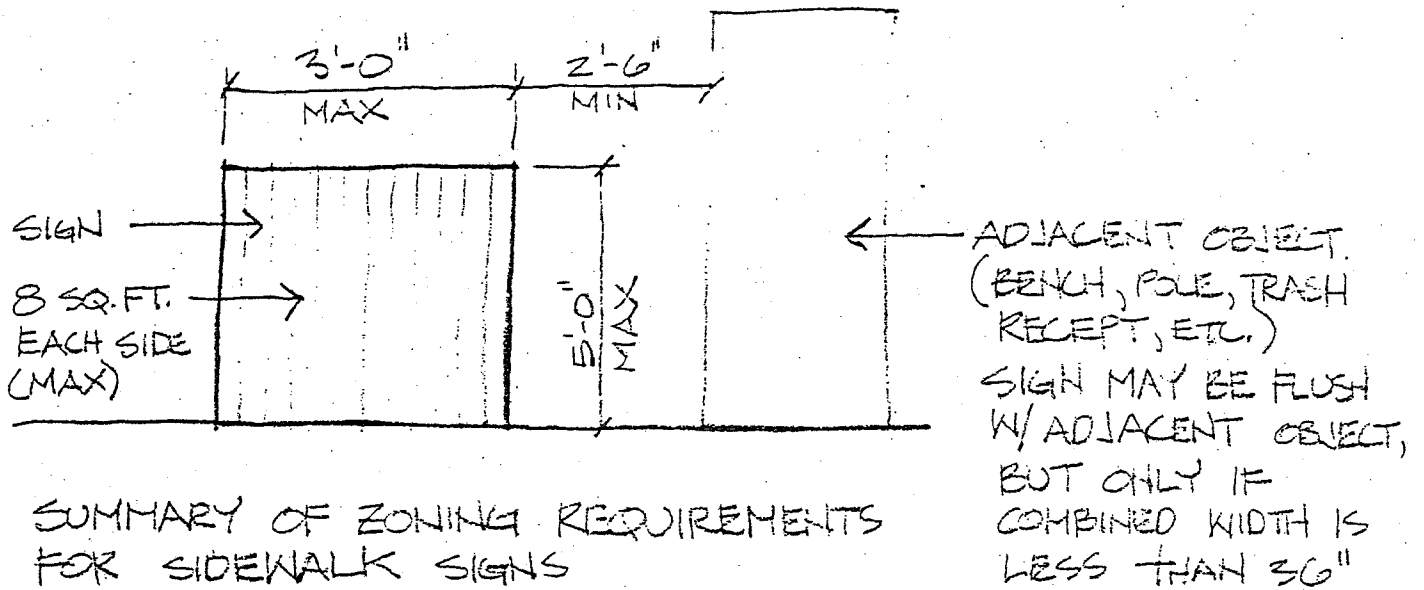
SIDEWALK

CURE

TYPICAL SIDEWALK PLAN

1/4" = 1'-0" ±

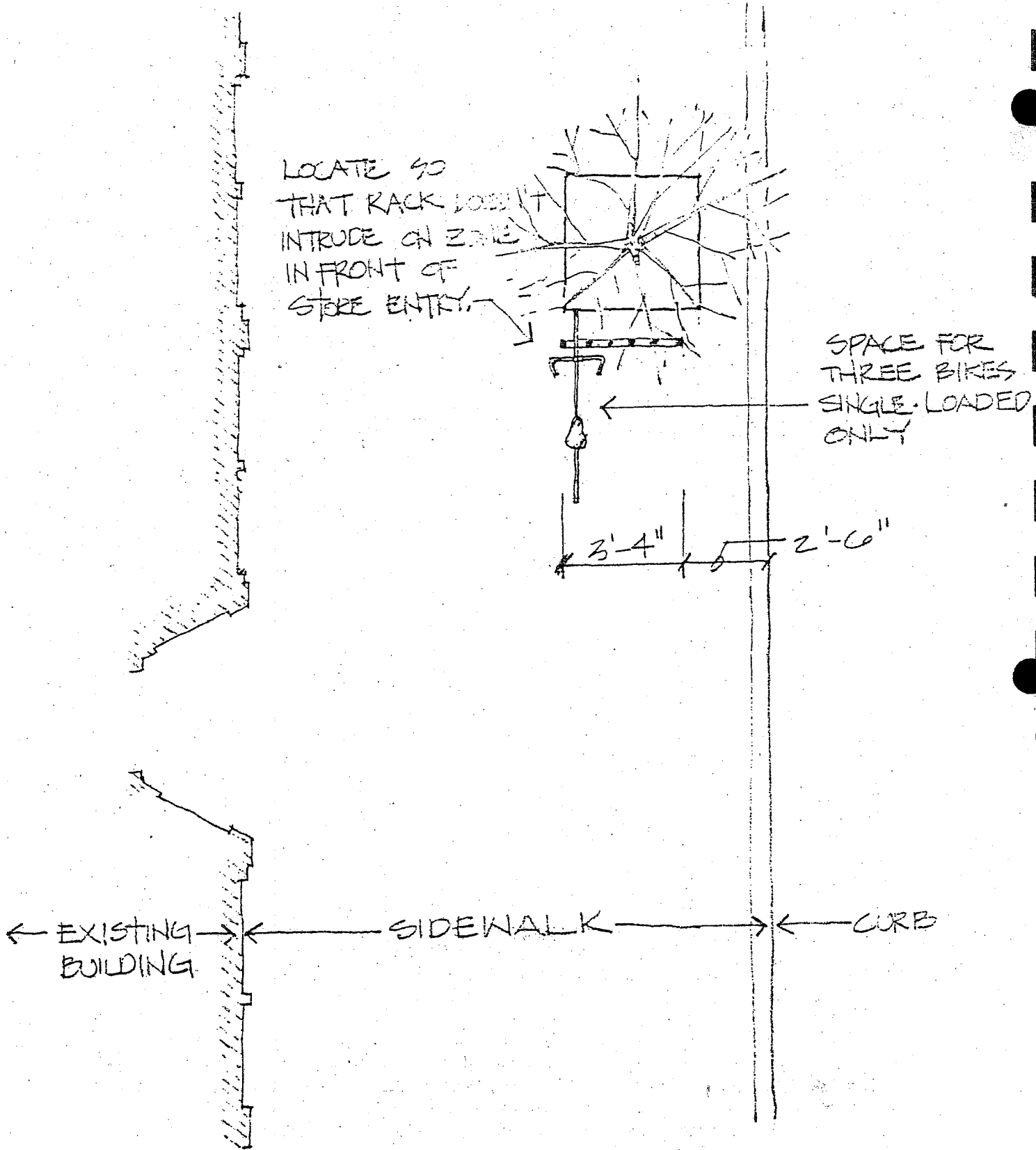
PLANTERS & SIDEWALK SIGNS



The current sign ordinance -- Section 17.34 of the zoning code -- treats sidewalk/sandwich signs adequately, except that we recommend reducing the total allowed combined width from 3-feet to 2-1/2-feet. It is important that these signs do not intrude on the pedestrian zone, as shown on the facing page.

Requirements for street planters are similar to the sign requirements in that (1) a minimum 30-inch space should be left between the planter and adjacent street objects, or (2) the planter may be flush with adjacent objects, but only if the total combined width is less than 2'-6". Planters should be portable, and not attached to the sidewalk. It is recommended that planters be less than 2'-6" wide, measured perpendicular to pedestrian traffic flow.

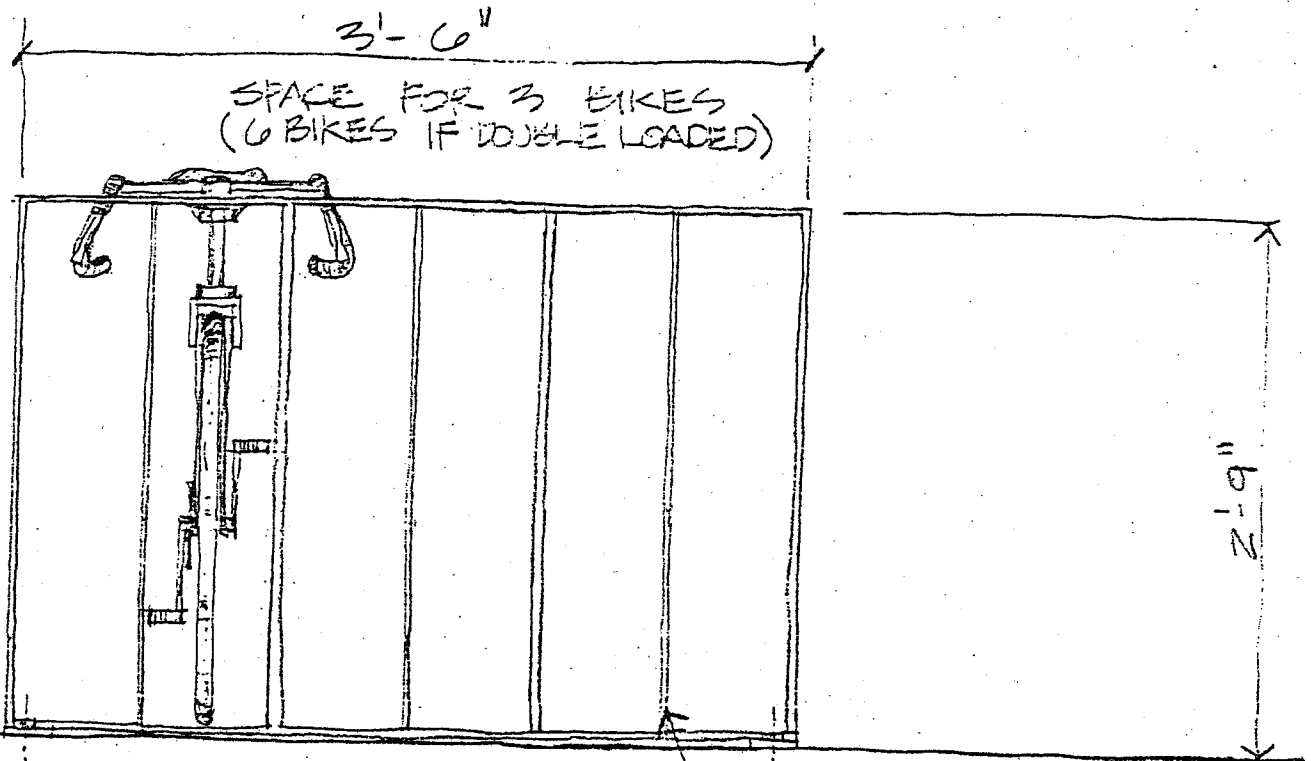
PLANTERS & SIDEWALK SIGNS



TYPICAL SIDEWALK PLAN

1/4" = 1'-0"

BICYCLE RACKS



ELEVATION
1" = 1'-0"

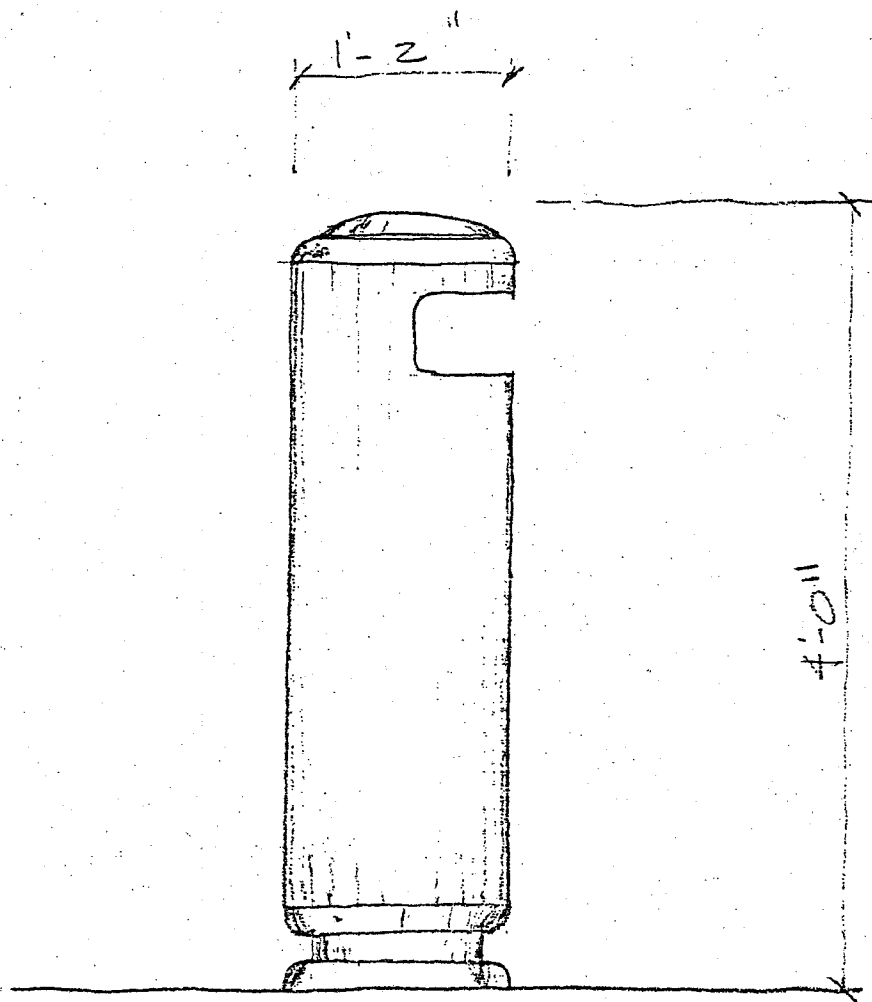
GALVANIZED
STEEL FRAME W/
DARK BRONZE PAINT

Ample bicycle racks are recommended to accommodate the large number of bicyclers in the district. The racks are simple and unobtrusive, and not too massive. They are placed perpendicular to the curb so that parked bicycles will not impede pedestrian traffic. The 3-foot 8-inch wide zone created by the tree grates is the appropriate space for the racks.

The bicycle rack shown above is manufactured by the Bike Security Racks Company of Cambridge, Massachusetts. It may be possible to fabricate a similar design locally for a comparable price. All of the available designs from current manufacturers are fairly modern in appearance. If a custom design were to be fabricated, it would be possible to attain some greater similarity of line and texture between the racks, benches, and other street furnishings.

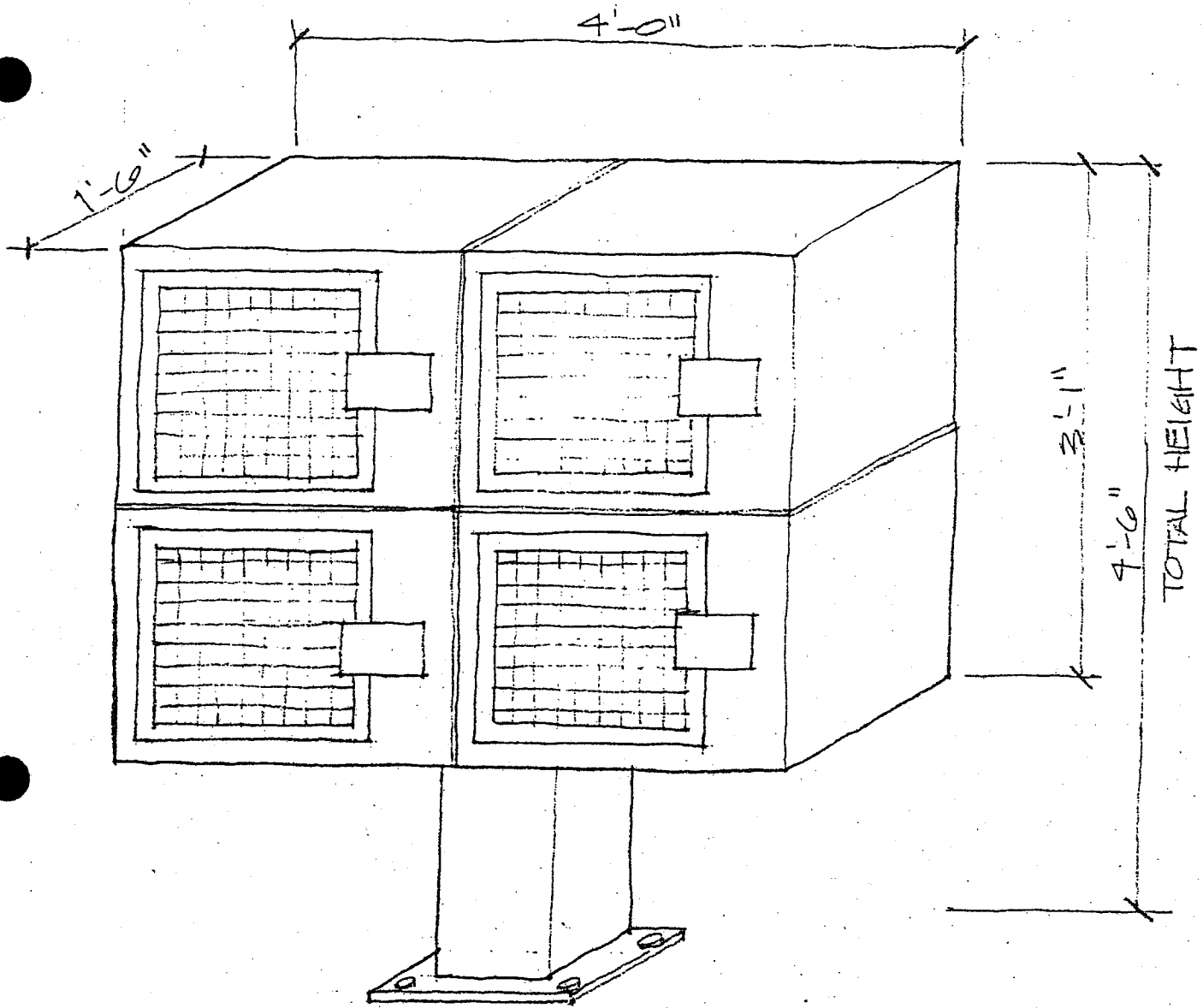
Price for Unit Shown: \$350 each, special order size
or 190 (each, for a stock 2-foot 8-inches wide unit, capable of accommodating two bicycles.)

BICYCLE RACKS



The newer Urban Accessories trash receptacles that were installed recently are of an appropriate design for the district. They are of a slender, simple, clean, and well-detailed design, and do not conflict with the architecture. Because of the small size of the can, ten more should be installed.

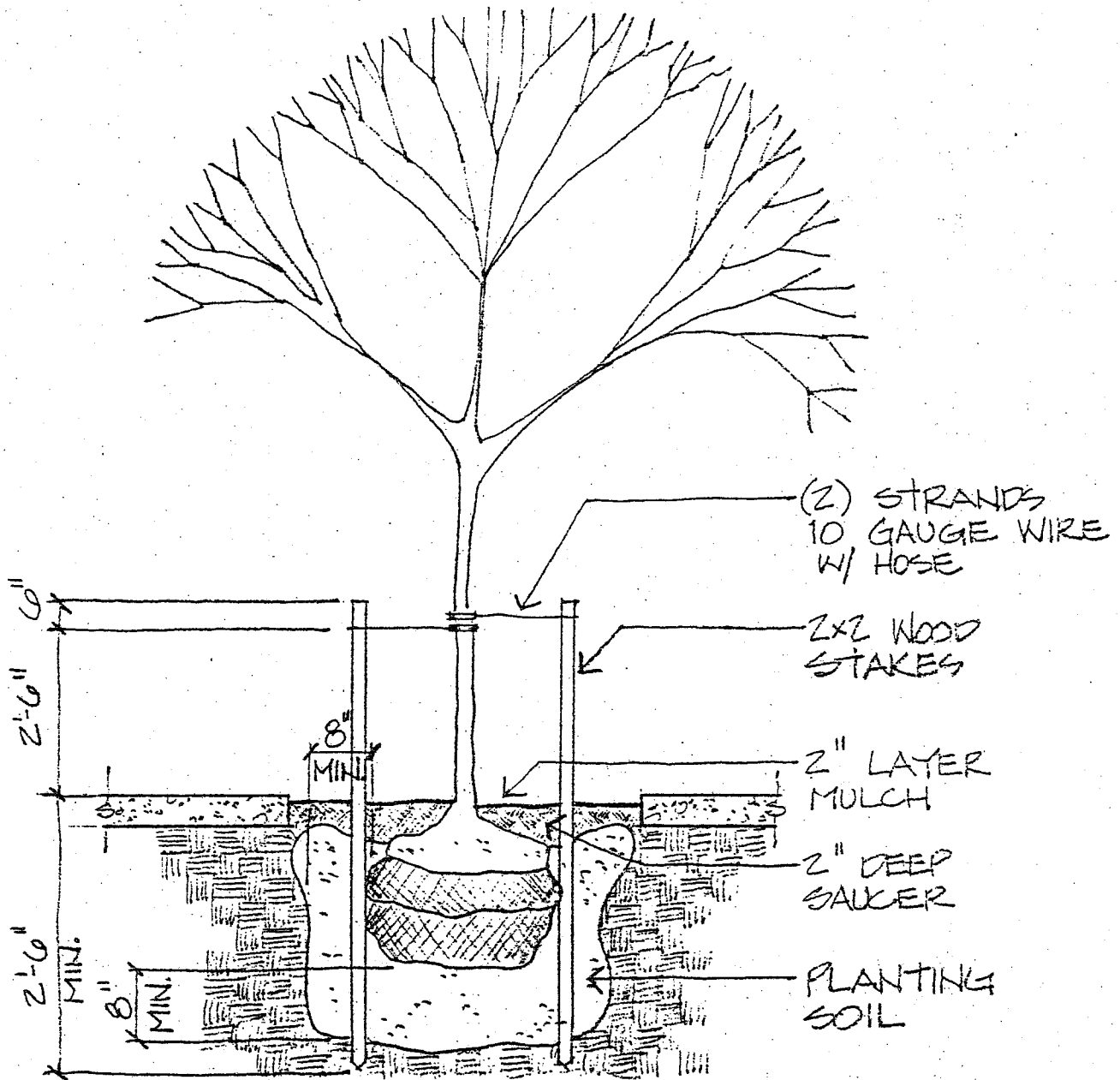
TRASH RECEPTACLES



Where there are three or more news racks at one location -- such as the Water and Taylor Street intersection -- it is appropriate to use the unit shown above. These modular news racks have proven to be very effective in various Seattle locations in saving space and reducing sidewalk clutter. They are manufactured by Sho-Rack.

Estimated Cost:	Typical Unit	\$300
	Shipping	15
	Install	60
	Total	\$375

NEWS RACK



STREET TREES

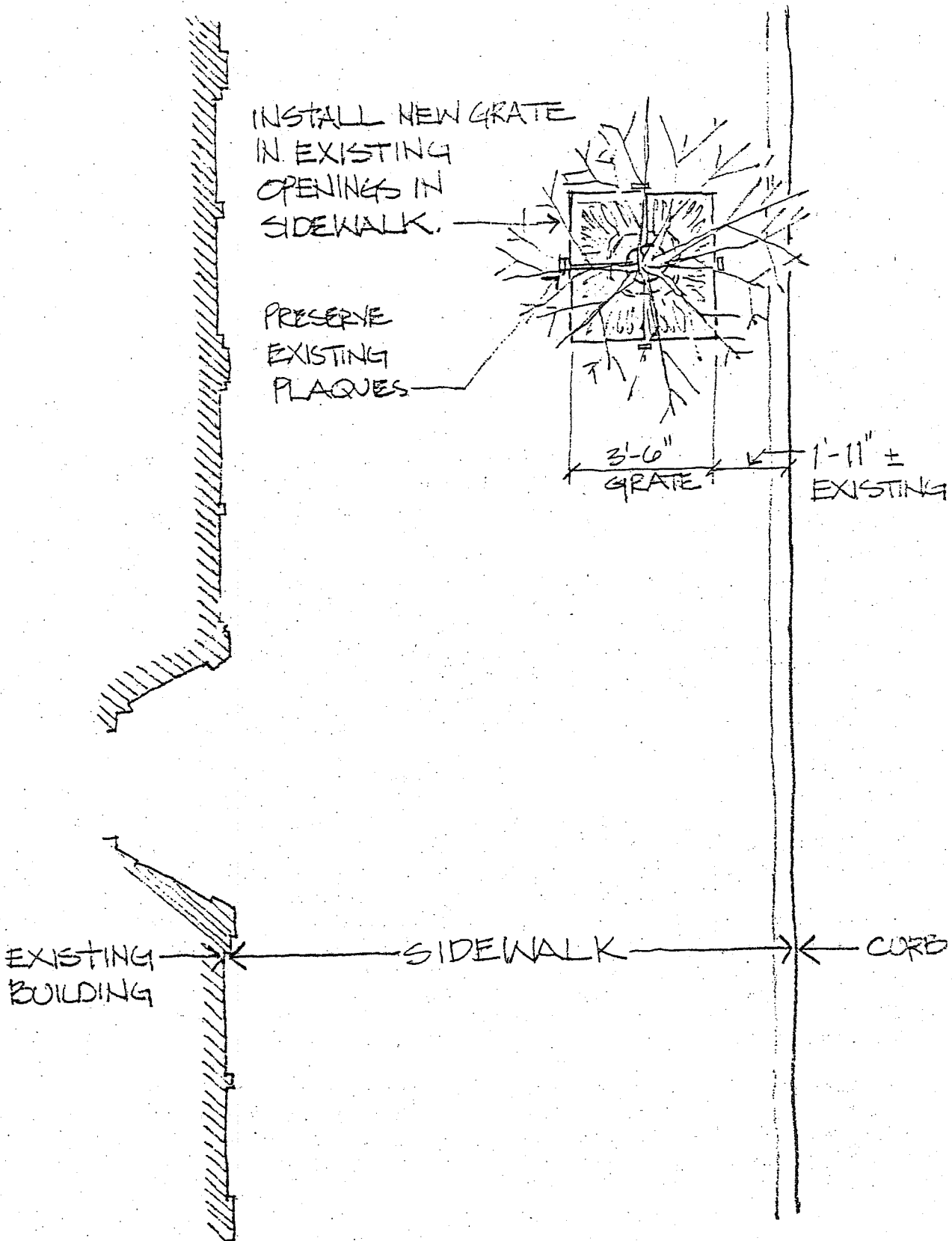
STREET TREE PLANTING

Street trees must be able to survive the wind and salt spray. The trees on Water Street are largely European Hornbeams, with Hawthorns in brick planters on the more protected cross streets. There are Norway Maples in the vicinity of Memorial Field and Monroe Street.

The trees should be three to four inches in diameter when planted. Saw cut a four foot by four foot square in the sidewalk centered 3'-8" from the curb and remove pavement. Excavate a plant pit sixteen inches wider and eight inches deeper than the tree container. Backfill with a planting soil consisting of 25% peat, 25% steer manure, and 50% native soil from the pit, well mixed. Place sufficient planting soil in the bottom of the planting pits so that, in relation to the surrounding grade, the tree will be one inch above the depth at which they were grown in the container. Carefully remove the container and set the tree without damaging the root ball. Superficially cut rootball on three sides using a knife. Set the tree plumb and in the center of the pits, and hold it in position until planting soil has been tamped firmly around the ball. Water and tamp each layer thoroughly to settle the soil. Place fertilizer consisting of 14% nitrogen, 14% phosphate, and 14% potassium evenly around the pit at the manufacturer's recommended rate. Construct a berm of topsoil to form a watering saucer two inches deep. Provide 2 inches of mulching materials consisting of shredded fir tree bark and bark peelings from 1/4 inch to 1-1/2 inches square. Stake the tree as in the drawing, using wood free from knots, rot, and cross grain. Use guying wire of 10 gauge annealed galvanized steel, with new or used two ply reinforced rubber or plastic hose as chafing guards. To survive, the trees must be watered weekly during dry times for three years.

The "Trees for Port Townsend Society," which planted most of the street trees in town for the last fifteen years, is available for advice on tree selection and planting procedures. The Parks Department has a map compiled by the Society of their tree plantings.

STREET TREES



TYPICAL SIDEWALK PLAN
 1/4" = 1'-0"

TREE GRATES

MANY TREES ARE OFF CENTER, SO THAT
BREAK-OUT RINGS WILL HAVE TO BE
REMOVED TO ACCOMMODATE THEM

EDGE OF EXISTING
OPENING IN CONC.

1" x 1" ANGLE

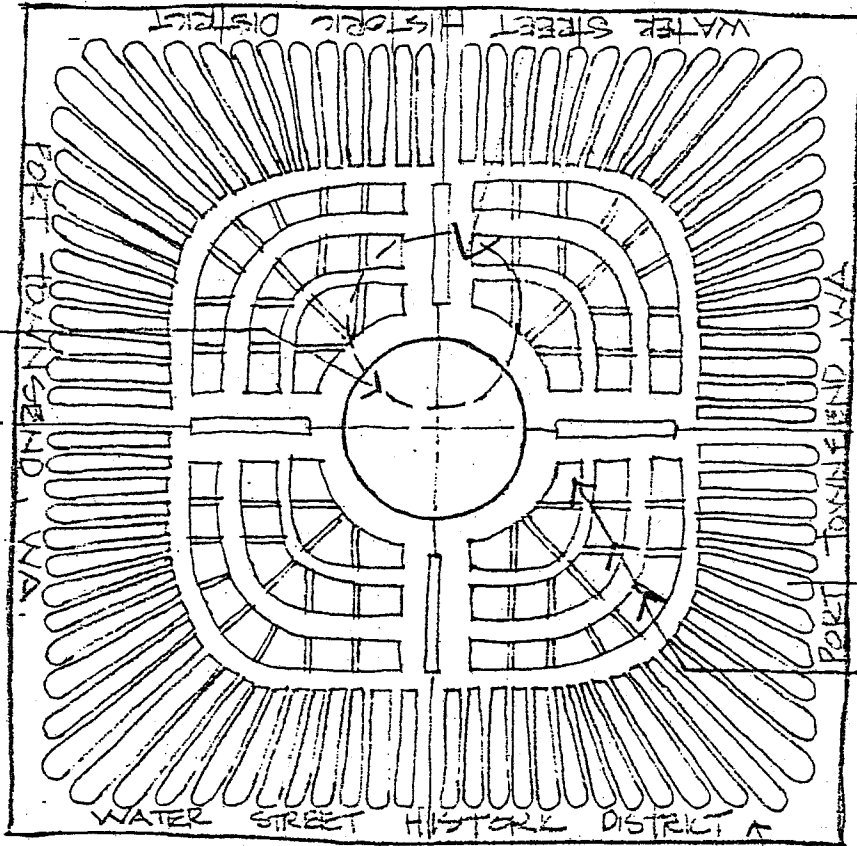
GRATE

WELD

EXP.
BOLT

DETAIL A

9" DIA.
OPENING



PLAN VIEW

BREAK-OUT RINGS
FOR TREE GROWTH

" x "

3/4" CAST IRON

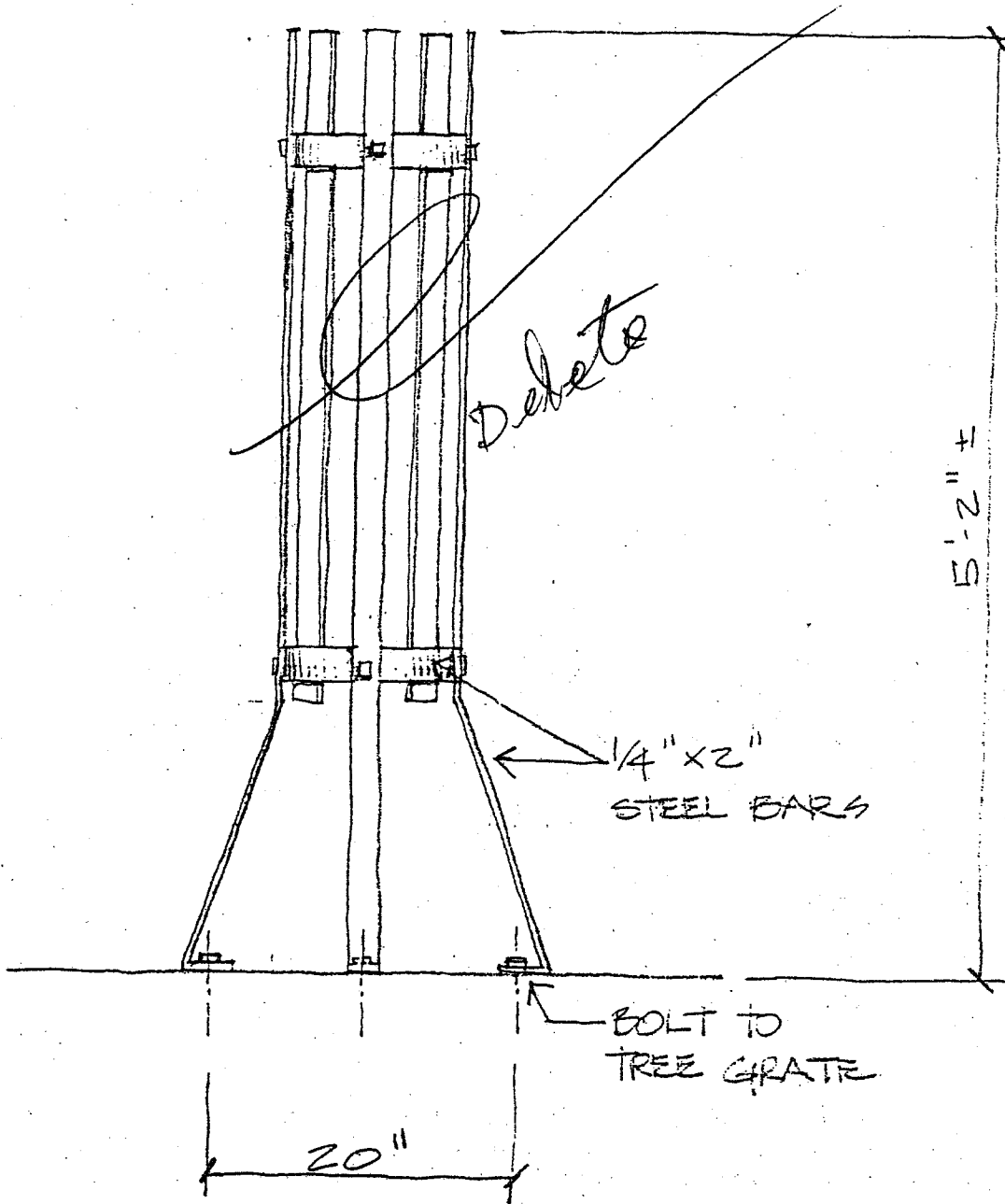
TAKE CARE TO
PRESERVE EXISTING
PLAQUES

Tree grates are suggested to protect the tree roots, beautify the pavement cut, and because they provide a means of establishing visual and textural continuity from block to block. They can be readily installed in the existing 3'-6" x 3'-6" sidewalk cuts. In some cases, break-out rings in the grates will need to be removed to accommodate the trees that are not centered in openings.

If grates are not provided, it is recommended that the city establish a uniform policy of planting and maintaining flower beds in the openings. There will always be a problem with maintaining these flower beds due to the closeness of the openings to the curb, and thus to traffic to and from parked cars.

Estimated Cost:	Grate	\$300	(Urban Accessories of Snohomish)
	Install	\$100	
	Total	\$400	

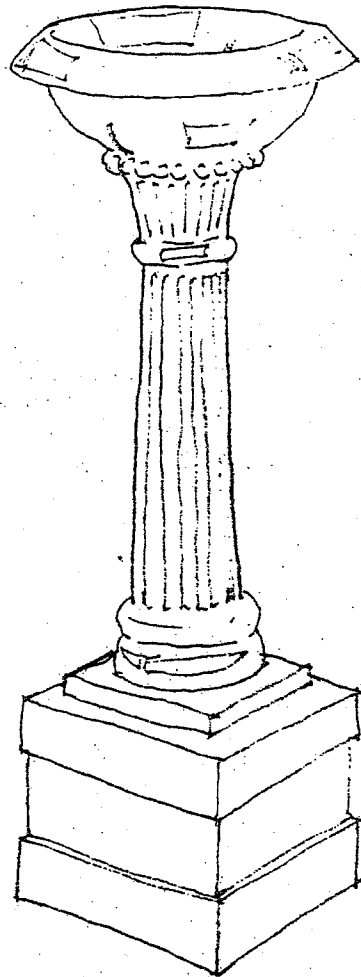
TREE GRATES



Tree guards are suggested -- budget permitting -- to protect the trees as well as to provide for visual and textural unity and continuity within the district. The guard shown is the "Economy" model by Urban Accessories, and is designed to be compatible with their tree grate design.

Estimated Price: \$120 each, primed and painted.

TREE GUARD

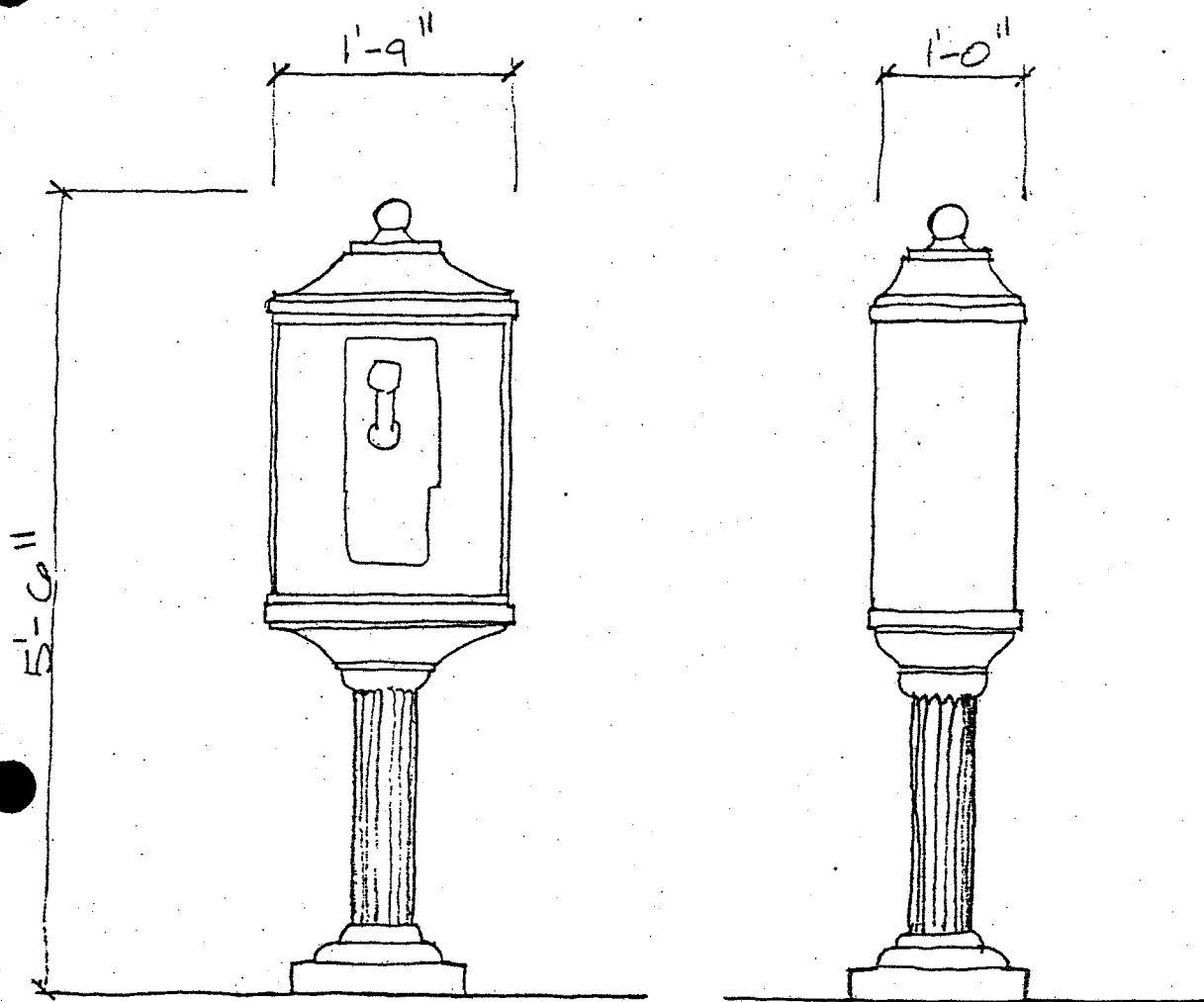


← 30" HEIGHT
CAST IRON W/
BRONZE BASIN
110 POUNDS

Two drinking fountains should be installed, budget permitting, with one located at the corner of Water and Taylor Street (see "typical Block Plan") and the other located at City Hall. The one shown is manufactured at the Olympic Foundry in Seattle.

Estimated Cost:	Fountain	\$1450
	Shipping	50
	<u>Install</u>	<u>250</u>
	Total	\$1750

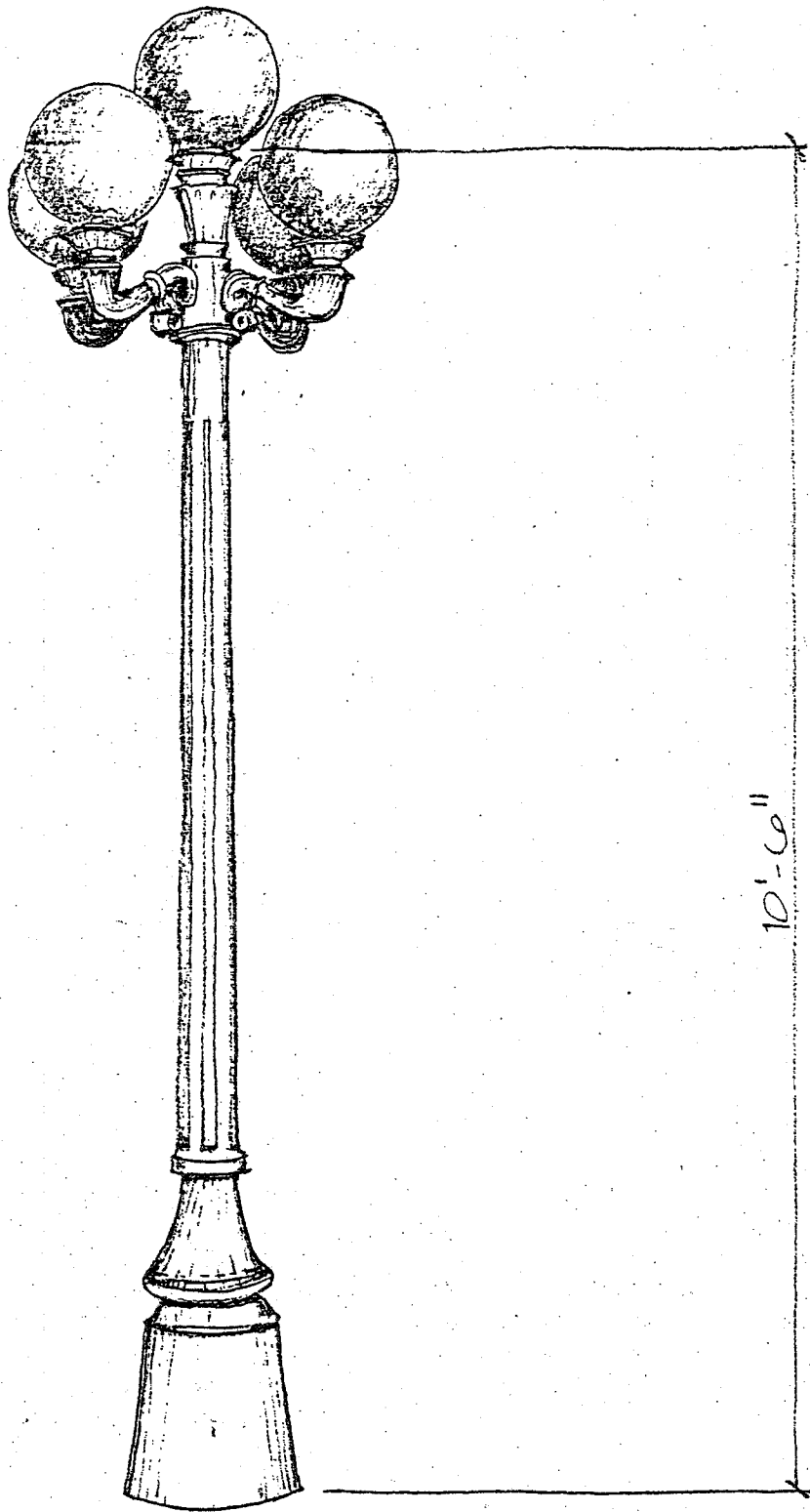
DRINKING FOUNTAIN



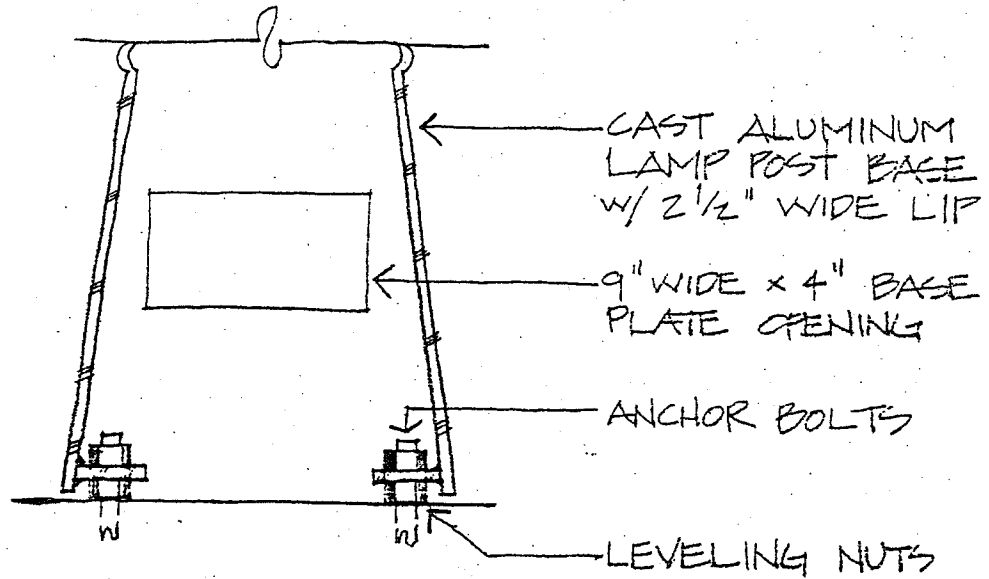
One of the difficulties in selecting street furnishings for a historic district is that certain furnishings -- such as phone booths and newspaper stands -- are often not included in the selection process. The result is a streetscape design that does not quite come together. The phone booth shown above is very successful in complementing the street furnishings in Seattle's Pioneer Square District. It is expensive; but if budget permits, it is recommended for two or three locations in the Water Street Historic District. The manufacturer is Phillips and Brooks, Inc.

Estimated Cost: \$1400 includes shipping,
not including phone and installation

TELEPHONE BOOTH



LAMP POSTS



SECTION THROUGH BASE

The lamp post shown is the cast aluminum type recently installed in the Port Townsend Historic District. Future street lamps should be the same type, and should be installed in the curbside zone, as described on page 6. The lamp is distributed by A-America in Seattle.

Estimated Cost:	Lamp post unit	\$300
	Shipping	\$ 20
	Installation	<u>\$400</u>
Total Cost:		\$720 (not including painting and wiring of lamp post)

LAMP POSTS

STREET SIGN FACE DESIGN	A-1
STANDARD HIGHWAY SIGN COLORS	A-2
STANDARD PARKING SIGN DESIGN	A-3
SUMMARY OF PRODUCTS RECOMMENDED	A-4

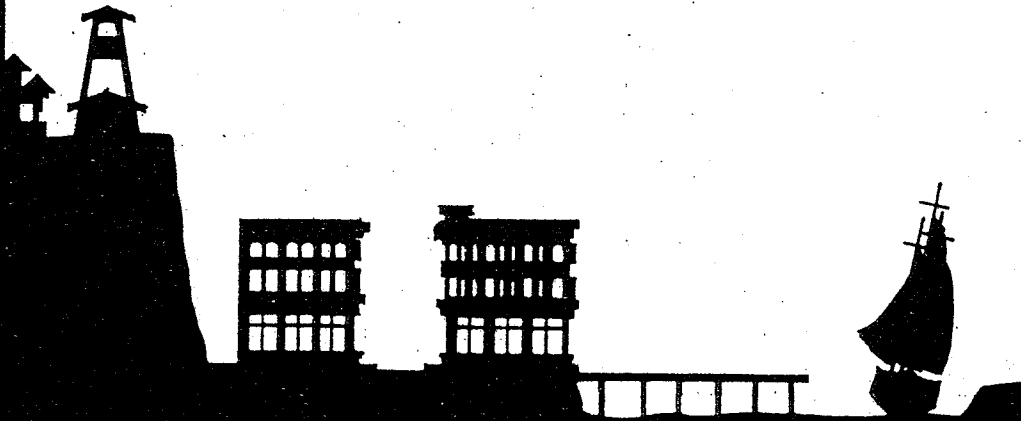
APPENDIX

2

**HR
PARKING**

**8 AM
- 5 PM**

EXCEPT SUNDAY



PORT TOWNSEND HISTORIC DISTRICT

Specifications for STANDARD HIGHWAY SIGN COLORS

This Data Sheet has been prepared by the Federal Highway Administration for the convenience of users and manufacturers of highway signs and to promote uniformity in sign colors. The Manual on Uniform Traffic Control Devices,¹ in sections 1A-8 and 2A-11, prescribe the color code for general meanings and color combinations for various classifications of highway signs. This sheet defines the six primary colors and specifies the tolerance for each color.

These Specifications for highway sign colors yellow, red, blue, green, brown, and orange have been taken from the Federal Highway Administration Color Tolerance Charts,² which contain color chips for each limit and the central color. The charts are designed to permit visual comparison of a sample with the standard colors.

PR Color #1 HIGHWAY YELLOW-13538*						
LIMIT	CIE DATA FOR SOURCE C			MUNSELL NOTATION		
	Y	x	y	H	V C	
Central	50.68	.5007	.4555	10YR	7.5/14	
Value+	59.10	.4940	.4530	10YR	8.0/14	
Value-	36.20	.5137	.4602	10YR	6.5/14	
Hue+	50.68	.4920	.4705	2.0Y	7.5/14	
Hue-	50.68	.5065	.4445	8.5YR	7.5/14	
Chroma+	50.68	.5133	.4631	10YR	7.5/16	
Chroma-	50.68	.4826	.4447	10YR	7.5/12	

PR Color #4 HIGHWAY GREEN-14109*						
LIMIT	CIE DATA FOR SOURCE C			MUNSELL NOTATION		
	Y	x	y	H	V C	
Central	6.56	.2088	.4101	7.5G	3.00/8	
Value+	10.43	.1942	.4228	7.5G	3.75/10	
Value-	3.82	.1904	.4208	7.5G	2.25/8	
Hue+	6.56	.1943	.3779	0.5BG	3.00/8	
Hue-	6.56	.2228	.4380	5.0G	3.00/8	
Chroma+	6.56	.1800	.4310	7.5G	3.00/10	
Chroma-	6.56	.2346	.3901	7.5G	3.00/6	

PR Color #2 HIGHWAY RED-11105*						
LIMIT	CIE DATA FOR SOURCE C			MUNSELL NOTATION		
	Y	x	y	H	V C	
Central	9.00	.6003	.3146	7.0R	3.5/13	
Value+	12.00	.5738	.3262	7.0R	4.0/13	
Value-	6.56	.6268	.3030	7.0R	3.0/13	
Hue+	9.00	.5928	.3235	8.0R	3.5/13	
Hue-	9.00	.5915	.3065	6.0R	3.5/13	
Chroma+	9.00	.6330	.3045	7.0R	3.5/15	
Chroma-	9.00	.5835	.3185	7.0R	3.5/12	

PR Color #5 HIGHWAY BROWN**						
LIMIT	CIE DATA FOR SOURCE C			MUNSELL NOTATION		
	Y	x	y	H	V C	
Central	5.52	.4766	.3816	5YR	2.75/5	
Value+	7.71	.4608	.3801	5YR	3.25/5	
Value-	3.81	.4955	.3826	5YR	2.25/5	
Hue+	5.52	.4762	.3981	7.0YR	2.75/5	
Hue-	5.52	.4728	.3607	2.5YR	2.75/5	
Chroma+	5.52	.5081	.3912	5YR	2.75/6	
Chroma-	5.52	.4450	.3720	5YR	2.75/4	

PR Color #3 HIGHWAY BLUE-15090*						
LIMIT	CIE DATA FOR SOURCE C			MUNSELL NOTATION		
	Y	x	y	H	V C	
Central	6.56	.1780	.1833	2.5PB	3.0//8	
Value+	9.00	.1887	.1963	2.5PB	3.5/8	
Value-	3.13	.1540	.1530	2.5PB	2.0/8	
Hue+	6.56	.1908	.1799	5.0PB	3.0/8	
Hue-	6.56	.1658	.1905	10B	3.0/8	
Chroma+	6.56	.1576	.1600	2.5PB	3.0/10	
Chroma-	6.56	.2022	.2101	2.5PB	3.0/6	

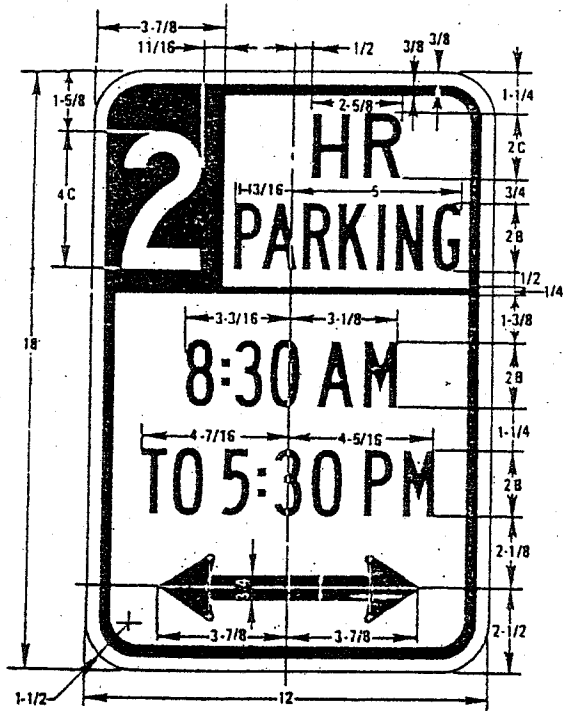
PR Color #6 HIGHWAY ORANGE***						
LIMIT	CIE DATA FOR SOURCE C			MUNSELL NOTATION		
	Y	x	y	H	V C	
Central	24.58	.5609	.3950	2.5YR	5.5/14	
Value+	30.05	.5488	.3947	2.5YR	6.0/14	
Value-	19.77	.5731	.3953	2.5YR	5.0/14	
Hue+	24.58	.5570	.4072	3.75YR	5.5/14	
Hue-	24.58	.5614	.3815	1.25YR	5.5/14	
Chroma+	24.58	.5713	.3970	2.5YR	5.5/15	
Chroma-	24.58	.5348	.3898	2.5YR	5.5/12	

* Color number in Federal Standard 595a meeting this specification.

** ISCC-NBS color designation 56-strong brown approximates this specification.

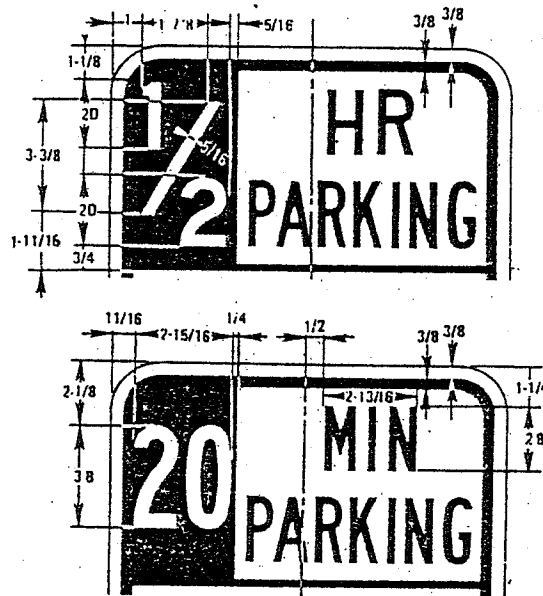
*** ISCC-NBS color designation 48-vivid orange approximates this specification.

1. *Manual on Uniform Traffic Control Devices for Streets and Highways*, 1978 edition, Federal Highway Administration. For sale by the Superintendent of Documents, U.S. Government Printing Office, Washington, D.C., 20402. Stock No. 050-001-90001-7. Price \$18.00 including subscription service for future changes.
2. *Color Tolerance Charts*, PR Colors 1-4 (June 1965), Colors 5-6 (March 1971), Federal Highway Administration, contact the Federal Highway Administration, Washington, D.C., 20590 for availability information.



R7-108

Note: Letter sizes shown will have to be changed; see page A.1.



R7-108 (Variations)

COLORS

LEGEND — GREEN
BACKGROUND — WHITE

Summary of Products Recommended

Benches:	Steve Lopes Port Townsend	385-5448
Trash Receptacles:	Urban Accessories Snohomish, WA	568-3143
Tree Grates:	Urban Accessories Snohomish, WA	568-3143
	Olympic Foundry Seattle, WA	764-6200
Tree Guards:	Urban Accessories Snohomish, WA	568-3143
Bike Racks:	Bike Security Racks Co. Cambridge, MA	(617) 547-5755
Drinking Fountain:	Olympic Foundry Seattle, WA	764-6200
Newspaper Racks:	Sho-Rack	(415) 584-7231 (Local Rep.) Henry Machen
Phone Shelter:	Philips and Brooks, Inc. Cummings, GE	(714) 533-9230 (Local Rep.) Tracy Armstrong Perry
Lamp Posts:	A-America	251-6345 or 467-7400 (Local Rep.) Dick Hawes